

# **PLANNING COMMITTEE ADDENDUM 1**

**2.00PM, WEDNESDAY, 10 JUNE 2020**

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## **ADDENDUM**

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Planning Committee 10 June 2020 – Member Questions

Deed of Variation for Anston House		
1	In what position of the building will these flats be situated? Will they enter in the same way as everyone else? Who and what will decide which flats are the affordable ones?	The affordable units will be interspersed with the other build to rent flats and will use the same entrance. The exact location of the affordable units is not specified and will be allocated by the operator of the development.
Item A: BH2020/00442 Black Rock		
1	If the existing use of Madeira Drive is to be retained for walking and cycling i.e. no vehicular access – how will that effect this development?	This is not relevant to the determination of this application.
2	How will this development impact on any future development of Madeira Terrace?	<p>The applicant has responded as follows: This scheme of enabling and public realm enhancement should have a significant positive effect upon the regeneration of Madeira Terrace, not least of which for the following reasons:</p> <ul style="list-style-type: none"> <li>• It will provide a catalyst of renewal and activity, in the short, medium and long term, which will encourage footfall and activity to the east of the Palace Pier towards the Marina;</li> <li>• The Madeira Terrace will therefore sit in the middle of the major attractors of Palace Pier to the west and Black Rock and the newly 'connected' Marina to the east, providing the ideal conditions for viable investment and reuse of the Madeira Terraces;</li> <li>• It will improve pedestrian and cycle connectivity to the east of Madeira</li> </ul>

		<p>Terrace;</p> <ul style="list-style-type: none"> <li>• It will improve public realm at the bottom of Duke's Mound around the Volks workshops, all of which standing in close proximity to the eastern end of the Madeira Terrace;</li> <li>• It will include restoration and reuse of the Temple and Old Reading Rooms, heritage assets along this section of the seafront to be considered alongside the Madeira Terraces;</li> <li>• It will also introduce the new boardwalk and ecology trail to the east and improvements to Kemp Town Slopes;</li> <li>• It will provide a cleared site to be used as a temporary events space within the city adding vibrancy and activity to the eastern seafront; and</li> <li>• In the longer term will facilitate the provision of a major new development opportunity of strategic importance.</li> </ul>
3	Is Councillor Rainey's comment included in the report?	The committee report makes reference to the submission of a comment by Cllr Rainey, with a copy of the comments sent to Members.
4	There is a photo of Black Rock LWS, but is it possible to see a map of the site with the LWS marked on it, to see exactly where and how big it is?	This is now included within the presentation.
5	Reading Room/Temple detail slides (29 and 30) - are there drawings on the planning application that we could have the links to?	<p><a href="https://planningapps.brighton-hove.gov.uk/online-applications/files/AB71A90148C72C37CDD423CD9EC3D4AA/pdf/BH2020_00442-THE_TEMPLE_DRAWINGS-15741380.pdf">https://planningapps.brighton-hove.gov.uk/online-applications/files/AB71A90148C72C37CDD423CD9EC3D4AA/pdf/BH2020_00442-THE_TEMPLE_DRAWINGS-15741380.pdf</a></p> <p><a href="https://planningapps.brighton-hove.gov.uk/online-applications/files/A5588C5BD68D0371554C14953246E37B/pdf/BH2020_00442-THE_OLD_READING_ROOM_DRAWINGS-15741379.pdf">https://planningapps.brighton-hove.gov.uk/online-applications/files/A5588C5BD68D0371554C14953246E37B/pdf/BH2020_00442-THE_OLD_READING_ROOM_DRAWINGS-15741379.pdf</a></p>
6	Is it possible to find out if BHCC is serious about making Madeira Drive car-free all year round? Or if it is considering changing the layout	This is not relevant to the determination of the application



	<p>permanently along its whole length - not just BR end - to make it more pedestrian/bike friendly? If so, this would impact the BR plans - and therefore maybe the application should be changed so that work on the sea wall, excavation, sports pitch etc could start (when permission is given).</p> <p>With the roads and transport issue addressed separately to take into account the long-term plan for Madeira Drive traffic.</p>	
7	<p>With the upcoming development of the GasWorks site, and an additional 1000 flats at the marina, can any changes to the A259 be delayed so that a total traffic plan for the area be compiled? Isn't there a legal requirement to consider cumulative impact of developments in an area?</p>	<p>National Planning Policy suggests that cumulative transport impacts should be considered when determining applications. Planning Practice Guidance advises likewise and further recommends that this should include impacts from committed developments and allocated sites (subject to certain provisos). Based on submitted information Officers are satisfied that these things have been considered.</p> <p>Works to the A259/Dukes Mound junction are being proposed by the applicant. Officers are satisfied that there is sufficient planning policy justification to recommend these.</p>
8	<p>The Traffic Management Act 2004. The duty on an LTA does not stop at its borders. Each is required to facilitate the expeditious flow of traffic on the networks of others. In practice, for an LTA this will mean identifying those authorities that could be affected by their actions and making arrangements for managing this, even though they</p>	<p>Given no objection from the Local Highway Authority regarding this issues, the Local Planning Authority has not directly consulted with ESCC in this instance.</p> <p>The trip generation for the construction phase can be provided as part of the Construction Environment Management Plan [CEMP], secured by Unilateral Undertaking.</p> <p>The Local Highway Authority have stated that despite the inherent stopping delays created by the proposed signal installations, it is considered that the benefits justify</p>

	<p>may not be immediately adjacent authorities.</p> <p>Has BHCC consulted with ESCC about the planned A259 changes at Dukes Mound as delays here will impact on the A259 to the east?</p>	<p>the provision of this infrastructure in terms of; allowing for increased turning movements by large and heavy goods vehicles, providing signalised pedestrian crossings, and providing an improved road network as part of the enabling works for the allocated future use of the Black Rock site. The proposed MOVA automated control system of the signals will ensure they operate as efficiently as possible.</p>
9	<p>Approximately 23 evening peak period buses (12s, 14s, 27s) travel East on the A259 through Dukes Mound carrying over 1,800 passengers. In our post-COVID19 world, bus capacity will be reduced initially by up to 90% but more realistically by up to 50% for several years with a corresponding increase in bike, motorbike and car vehicles of up to 900 per peak hour. A similar situation applies to the morning traffic.</p> <p>The traffic assessments on this major trunk road are therefore incorrect including tables 7.4, 7.5, and 7.6 which record pedestrian and cyclist movements.</p> <p>Surely these figures must be re-done to reflect the current/future picture accurately?</p> <p>Should the business case be amended</p>	<p>Transport Assessments are undertaken based on evidence of historic trends and norms. Whilst various pandemic-related emergency measures are currently in place following national guidance (including emergency changes to bus timetables and restrictions on occupancy) these are temporary only to cover the unprecedented circumstances. It would not be reasonable to assume these yet to constitute a norm.</p> <p>The broader issue of a business case is not relevant to the determination of this planning application and is a matter for the applicant and any funders.</p>

	to reflect the increased disbenefits resulting from increased bus, motorbike and car traffic?	
10	<p>There is a groundswell of support towards making Madeira Drive permanently car-free. How would this impact upon the traffic assessments for Black Rock?</p> <p>Should we consider the likelihood of the temporary pedestrianisation of Madeira Drive becoming permanent to endorse B&amp;HCC's stated objectives of encouraging walking and cycling? Thus, should we defer consideration of Black Rock until a future meeting to ensure clarification of all undetermined considerations before making an irreversible decision?</p>	This is irrelevant to the determination of this application
11	If Madeira Drive becomes car-free, then the journeys outlined in table 7.1 will be diverted from Madeira Drive to the A259, how will this impact upon Valley Gardens?	There is no current committed scheme to make Madeira Drive permanently car free. No such proposal is included in this application. The question is therefore not relevant to the determination of this application.
12	Point 7.5.2 of Stantec report - <i>"Given the nature of the proposals being put forward as part of Valley Gardens, we do not anticipate there to be a significant change in traffic flow through the Duke's Mound junctions, assuming that Madeira Drive is retained as per its</i>	Section 9.5 of the submitted Transport Assessment includes modelled sensitivity tests that consider potential future changes to Madeira Drive – includes making this one-way eastbound only between Palace Pier and Dukes Mound per the committed Valley Garden's improvement scheme.

	<p><i>existing two- way arrangement”.</i></p> <p>Current plans make Madeira Drive one-way going east. Is this therefore a false anticipation?</p>	
13	<p>The Stantec Transport assessment of January 2020.</p> <p><u>The Coastal Transit Road Scheme - section 3.5.</u> This is a sustainable transport route linking into the marina from Madeira Drive West. Section 3.7.3 says that due to Valley Gardens and events on Madeira Drive, Duke’s Mound will be the main access to and from the Black Rock site. How will the CTR link route work then?</p>	<p>Introduction of the CTR service itself is not proposed in this application. Rather, the application includes the creation of infrastructure that could be used should such a service be introduced by others in future. This includes a new link road into the Marina along the northern edge of the Black Rock site. This aligns broadly with the route alignments identified in PAN04 (Brighton Marina Masterplan) and safeguarded in other council decisions. These provide for a primary route from the City Centre to the Marina via Edwards St or Marine Parade and Marina Way, and a secondary route via Madeira Drive. The alignment envisaged in this application is that the secondary route would pass via Marine Parade (west of Dukes Mound), Dukes Mound, Madeira Drive (east of Dukes Mound), and the proposed CTR link road behind the Black Rock site into the Marina, returning the same way.</p>
14	<p>The Stantec Transport Assessment of January 2020</p> <p>point 5.1.3 - The introduction of traffic lights at Dukes Mound is due to a desire to enable HGVs to be able to turn west from Madeira Drive into Marine Parade. Why do we want HGVs going into the city centre - or further west along the seafront? The most direct route out of the city is via Wilson Avenue.</p>	<p>This left turn is required to cater for a range of large vehicle movements, including buses and private coaches, many of which will have a need to directly access the city centre. The geometric change is compatible with traffic changes proposed under the committed Valley Gardens scheme to make the Madeira Drive east-bound only between Palace Pier and Duke Mound. However, it is not reliant on this.</p>
15	<p>The Stantec Transport Assessment of January 2020</p>	<p>The western end of Madeira Drive experiences significantly greater footfall and activity. The Local Highway Authority therefore supports proposals to route construction traffic via Dukes Mound to minimise the potential for conflict. However,</p>

	point 5.2.2 - for traffic coming from the west, why isn't Madeira Drive at the aquarium roundabout the access point rather than Dukes Mound? There is less traffic and there are no homes along Madeira Drive.	per recommended condition 3, final construction traffic routing would be determined through a CEMP.
16	<p>The Stantec Transport Assessment of January 2020</p> <p>Point 6.3.1 - <i>"there is no formal interim bus routes proposed for the seafront, however, provision of the new link will facilitate these in the future". Bridget Fishleigh has already asked B&amp;H bus company to provide a route along Madeira Drive (possibly by diverting some 27 buses) and this has been refused. Have B&amp;H buses been consulted on route viability?</i></p>	As the question recognises, no new bus services are proposed in the application (whether interim or permanent). Therefore, there is no purpose in consulting B&H buses or other operators at this time.
17	<p>The Stantec Transport Assessment of January 2020</p> <p>Section 9.1.6 - <i>"the signalised pedestrian crossings have been modelled being called up every other cycle".</i></p> <p>This technique for boosting A259 junction capacity has been rejected by B&amp;H in the past on road safety grounds, as bikes, cars and</p>	<p>Transport Officers were unable to recommend that the proposed designs to signalise these junctions were secured as set out in the application, but were satisfied with the principal of signalising them. Following their advice a highway design condition in a 'Plans Notwithstanding' format (condition 15) has been recommended to allow the proposed arrangements to be amended under a future Approval of Details application. This would include the detailed signalling arrangements. Amongst other things, the condition would require the proposed design and signalling arrangements to complete further, more detailed, road safety audit.</p> <p>In practice, actual signalling and control arrangements will differ from those that have been assumed for modelling purposes in this application. Those assumptions</p>

	<p>pedestrians anticipating a “go ahead” cycle that does not happen may enter the junction unsafely.</p> <p>Please provide documentation showing this potentially dangerous sequence was explicitly approved by the Traffic Signals Officer and Road Safety officer.</p> <p>The saturation flows for the A259 Exit lanes used in the traffic model are higher than would be expected given the queues observed at peak times, which would inflate substantially the junction capacity.</p> <p>Please confirm these flows were measured rather than estimated and provide details on how and when this was done.</p>	<p>reflect in part the limitations of the modelling software. The TA proposes that junctions would operate under MOVA control. This allows for automated real time adjustments and prioritisation of different movements in response to live information about traffic demand. Final configuration and settings would be approved by BHCC’s Traffic Control Team.</p> <p>It is not possible to measure saturation flows for the purposes of modelling the proposals to signalise the Dukes Mound junctions. This is because there are no signals presently.</p>
18	<p>The Stantec Transport Assessment of January 2020</p> <p>Section 9.3.10. The queue of vehicles is given as 42 with a delay of 40 seconds.</p> <p>This is the modelled average for the increase in queue during the peak hour (LinSig MMQ), however when estimating the length of the queue that</p>	<p>Transport Officers are satisfied that they have interpreted modelled queue and delay factors correctly when assessing the impacts of proposals. Given both the model input and outputs, and traffic flow survey data for different periods, they do not agree with the method proposed in the question for incorporating residual queues and delays from preceding peak hours.</p> <p>The broader issue of a business case is not relevant to the determination of this planning application and is a matter for the applicant and any funders.</p>

might obstruct other traffic, I understand that the maximum (84) should be used plus any queue that persists from the previous hour which gives 125 vehicles.

This is fundamental to assessing the congestion at the point where Westbound A259 traffic merges with traffic from the Marina.

Similarly delays must include the delay due to the residual queue which raises the delay to about 90 seconds.

This is a vital element in any business case appraisal that makes provision for the cost of people delays in accordance with the DfTs appraisal methodology (or Highways England HE PAR methodology) to obtain public funding.

Please confirm the congestion impacts analyses and any business case appraisal have taken into account these cumulative effects that the DfT and HE say are too often ignored.

Item B:  
BH2020/00325  
Avon Court

1	Is the cycle storage under the stairs adequate?	The cycle storage is considered to be adequate given that it would be secure, dry and for one bicycle only. A condition is recommended to ensure that this space is fit for use as cycle storage prior to the first occupation of the flat.
<p>Item C: BH2020/00947 Varndean College, Surrenden Road</p>		
1	Am I right in saying that classrooms 48-50 are required to be moved anyway by July 2024? That's how I'm reading slide 9.	Yes, that is correct. Temporary Classroom 48-50 relates to one of the temporary structures on site. There is a proposed condition (on the late list) that requires the removal of classroom 48-50 by 19 July 2024. This is in line with planning permission ref: BH2019/01385 which requires that the other remaining temporary classrooms are removed from site by the same date.
<p>Item E: BH2020/00187 29 Woodbourne Avenue</p>		
1	Can you explain what Councillor Wares is referring to in terms of previous enforcement regarding 29 Woodbourne Avenue?	There is a current enforcement case on this site which will be reviewed once this application has been determined. Councillor Wares has stated in his letter that if the issue and circumstances of the enforcement notice had not occurred then the applicant most probably would have been able to clad the side gable and rear dormer under permitted development. The use of cladding to the side gable and rear dormer is not considered to be permitted development as this fails to meet conditions set out in paragraph B.2 (a) of Schedule 2, Part 1 of the General Permitted Development Order (GDPO) which states 'the materials used in any exterior work must be of a similar appearance to those used in the construction of the exterior of the existing dwellinghouse'.
<p>Item F:</p>		



BH2020/00694  
105 Woodland Drive

1	Why is the footpath requirement an informative and not a condition?	<p>As part of the proposal pedestrian crossing improvements are required, namely the installation of dropped kerbs with paving and tactile paving at the junction of and across Hill Brow with Woodland Drive. This is to create the necessary pedestrian and mobility and visually impaired access to and from the site to the Dyke Road Avenue bus services to and from the City Centre and beyond regardless of which access they use.</p> <p>Condition 8 relates to such pedestrian crossing improvements and requires that;</p> <p>“Prior to first occupation of the development hereby permitted, pedestrian crossing improvements (dropped kerbs with paving and tactile paving) shall have been installed at the junction of and across Hill Brow with Woodland Drive.</p> <p>Reason: To ensure that suitable footway provision is provided to and from the development and to comply with policies TR7, TR11 and TR12 of the Brighton &amp; Hove Local Plan and CP9 of the Brighton &amp; Hove City Plan Part One”</p> <p>Informative number 3 is to be read in relation to condition number 3.</p>
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Item H:  
BH2020/00724  
2 Dyke Close

1	Can you clarify where the representations have come from?	<p>Of the representations received, residents representing four properties from the immediate vicinity of Dyke Close have objected to the proposal. Residents from two properties in Dyke Close support the scheme.</p> <p>Other representations in support were from Hill Brow and Dyke Road Avenue.</p>
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		Representations objecting to the scheme were also received from Hill Brow.
<p>Item I: BH2020/00776 6 Princes Crescent</p>		
1	Could we have a couple of visuals showing the extant permission, as the point is being made that the current proposal is very similar to it?	Comparative elevations will be added to the presentation
<p>Item J: BH2020/00235 39 Guildford Road</p>		
1	Is this part retrospective?	When the site was visited by the planning officer in February 2020 renovation works were being carried out. There was no evidence of any works being carried out which would require planning permission. The building was unoccupied. Council Tax records suggest that the building remains unoccupied.
2	Is there any alteration to the outside of the building?	The only external alteration on the proposed drawings is the insertion of a small vent at first floor on the rear elevation. This is a minor alteration, and not the sort of addition which would normally require permission, other than on a Listed Building, which this is not.
3	Can it be conditioned to be car free?	The previous occupiers of the upper floors would have had access to resident parking permits. It is not considered reasonable to restrict access to permits when there was no such restriction previously, and when the level of occupation would not be significantly greater. The Controlled Parking Zone (CPZ) offers its own restrictions, in so far as no further parking permits are issued when a CPZ reaches capacity.
4	This is in the West Hill Conservation Area. Should that be mentioned in the	No external alterations are proposed which would require planning permission. The situation of the building within the West Hill Conservation Area does not affect

	report?	whether the principle of the change of use would be acceptable. The application was correctly advertised as being within a Conservation Area and the late list has been updated.
Item K: BH2020/00791 47 Eley Drive		
1	Does an officer linked application mean it belongs to an officer or one of their family?	Yes, that is correct.

Add questions for Dyke Close



# 10<sup>th</sup> June 2020 Planning Committee – Additional Representations

Item	Site Address	Application No.	Comment
A	Black Rock Site And Surroundings Madeira Drive Brighton	BH2020/00442	<p><b>Additional Information Submitted by Applicant</b> Further data information on background traffic to be read in conjunction with the already submitted Transport Assessment.</p> <p>The Highway Authority has confirmed that cumulative impacts have been considered in the application. Based on submitted information Officers are satisfied that these things have been considered.</p> <p><b>Action</b> – None</p> <p><b>Comment from City Regeneration</b> City Regeneration has no adverse comments regarding this application.</p> <p>Due to the nature of this application which focuses on infrastructure and environmental improvements, Economic Development do not view it necessary to comment in any detail however, it is recognised that the various phases of the project have the potential to provide opportunities for employment and training for local residents.</p> <p>It should therefore be a requirement that an Employment &amp; Training Strategy be submitted in prior to formal project commencement. Fuller details are included in the Main Comments section</p> <p><b>Action</b> – The applicant has agreed to provide this as part of a Unilateral Undertaking</p> <p><b>Further comment from Environmental Health</b> Environmental Health recommend conditions</p> <p><b>Action</b> - Recommendation of amendment to Condition 11 (Land Contamination) and</p>

			<p>Informative 3 as set out below.</p> <p><b>One (1) Additional Letter of Representation submitted, objecting to the application as follows:</b></p> <ul style="list-style-type: none"><li>- Development for this road shouldn't be supported, as since it has been closed due to Covid-19, the space is more busy and there is a new demographic positively using the space (e.g older people on bikes)</li><li>- Many will not feel comfortable cycling on with cars and this threatens to roll back this positive change.</li><li>- Why is there a new road proposed into the marina - what is wrong with the existing network. It is not at capacity - can this development be called sustainable?</li><li>- Something needs to be done with the space, but a new road is not the answer.</li></ul> <p><b>Action –</b> The Highway Authority have commented fully on the application and in relation to the issues raised. The closure of Madeira Drive is a temporary measure. The proposal provides improved cycle infrastructure. The proposed Marina Access link would provide improvements to pedestrian and cycle routes, and future vehicle link as set out in the committee report.</p> <p><b>Conditions and Informatives</b> Addition to Condition 1 drawings table: 10856.REP.801 Design &amp; Access Statement</p> <p>Following clarification from the applicant on the phasing of works, the conditions below require the following amendments:</p> <p><b>Action –</b> see below</p> <p><b>Condition 1. –</b> to also include:</p> <table><tr><td>Design &amp; Access Statement</td><td>10856.REP.801</td><td></td><td>11 February 2020</td></tr></table>	Design & Access Statement	10856.REP.801		11 February 2020
Design & Access Statement	10856.REP.801		11 February 2020				

**Condition 3.**

(a) No development, including demolition, shall take place until an initial Construction Environmental Management Plan (CEMP) relating to the Duke's Mound Works has been submitted to and approved in writing by the Local Planning Authority.

(b) With the exception of the Duke's Mound Works, no development, including demolition, shall take place until a Construction Environmental Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority.

The CEMP shall include:

- (i) The phases of the Proposed Development including the forecasted completion date(s)
- (ii) A commitment to apply to the Council for prior consent under the Control of Pollution Act 1974 and not to Commence Development until such consent has been obtained
- (iii) A scheme of how the contractors will liaise with local residents to ensure that residents are kept aware of site progress and how any complaints will be dealt with reviewed and recorded (including details of any considerate constructor or similar scheme)
- (iv) A scheme of how the contractors will minimise disturbance to neighbours regarding issues such as noise and dust management vibration site traffic and deliveries to and from the site
- (v) Details of hours of construction including all associated vehicular movements
- (vi) Details of the construction compound
- (vii) A plan showing construction traffic routes
- (viii) In respect of Biodiversity:
  - a) risk assessment of potentially damaging construction activities;
  - b) identification of "biodiversity protection zones";
  - c) practical measures (both physical measures and sensitive working practices) to

			<p>avoid or reduce impacts during construction (may be provided as a set of method statements);</p> <p>d) the location and timing of sensitive works to avoid harm to biodiversity features;</p> <p>e) the times during construction when specialist ecologists need to be present on site to oversee works;</p> <p>f) responsible persons and lines of communication;</p> <p>g) the role and responsibilities on site of an ecological clerk of works (ECoW) or similarly competent person;</p> <p>h) use of protective fences, exclusion barriers and warning signs. The approved CEMP shall be adhered to and implemented throughout the construction period in accordance with the approved details.</p> <p>The construction shall be carried out in accordance with the approved CEMP.</p> <p>Reason: As this matter is fundamental to the protection of amenity, highway safety and managing waste throughout development works and to comply with policies QD27, SU9, SU10 and TR7 of the Brighton &amp; Hove Local Plan, policy CP8 of the Brighton &amp; Hove City Plan Part One, and WMP3d of the East Sussex, South Downs and Brighton &amp; Hove Waste and Minerals Local Plan 2013 and Supplementary Planning Document 03 Construction and Demolition Waste. To ensure that any adverse environmental impacts of development activities are mitigated and to avoid an offence under relevant wildlife legislation.</p> <p><b>Condition 8.</b> <u>No development shall take place in respect of the works impacting on the vegetated shingle hereby approved</u> until an ecological design strategy (EDS) addressing enhancement of the site for biodiversity, to include the recommendations in the Ecological Impact Assessment (Mott Macdonald, 31/01/2020), has been submitted to and approved in writing by the local planning authority. The EDS shall include the following:</p> <p>a) purpose and conservation objectives for the proposed works;</p> <p>b) review of site potential and constraints;</p> <p>c) detailed design(s) and/or working method(s) to achieve stated objectives;</p> <p>d) extent and location /area of proposed works on appropriate scale maps and plans;</p>
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			<p>e) type and source of materials to be used where appropriate, e.g. native species of local provenance;</p> <p>f) timetable for implementation demonstrating that works are aligned with the proposed phasing of development;</p> <p>g) persons responsible for implementing the works;</p> <p>h) details of initial aftercare and long-term maintenance;</p> <p>i) details for monitoring and remedial measures;</p> <p>j) details for disposal of any wastes arising from works.</p> <p>The EDS shall be implemented in accordance with the approved details and all features shall be retained in that manner thereafter.</p> <p>Reason: To ensure that any adverse environmental impacts of development activities can be mitigated, compensated and restored and that the proposed design, specification and implementation can demonstrate this, and to provide a net gain for biodiversity as required by Section 40 of the Natural Environment and Rural Communities Act 2006, paragraphs 170 and 175 of the National Planning Policy Framework, and Policies CP10 and DA2 of the Brighton &amp; Hove City Council City Plan Part One.</p> <p><b>Condition 9.</b> A landscape and ecological management plan (LEMP) for the creation and long-term management of at least 1.54 hectares of coastal vegetated shingle habitat and the management of existing vegetated shingle habitat within the Volk's Railway LWS, shall be submitted to, and approved in writing by, the local planning authority <u>prior to completion of the creation and fencing of the new shingle mounds for the coastal vegetated shingle habitat hereby approved</u>. The content of the LEMP shall include the following:</p> <p>a) description and evaluation of features to be managed;</p> <p>b) ecological trends and constraints on site that might influence management;</p> <p>c) aims and objectives of management;</p> <p>d) appropriate management options for achieving aims and objectives;</p> <p>e) prescriptions for management actions, together with a plan of management compartments;</p> <p>f) preparation of a work schedule (including an annual work plan capable of being</p>
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			<p>rolled forward over a five-year period;</p> <p>g) details of the body or organisation responsible for implementation of the plan;</p> <p>h) ongoing monitoring and remedial measures.</p> <p>The LEMP shall also include details of the legal and funding mechanism(s) by which the long-term implementation of the plan will be secured by the developer with the management body(ies) responsible for its delivery. The plans shall also set out (where the results from monitoring show that conservation aims and objectives of the LEMP are not being met) how contingencies and/or remedial action will be identified, agreed and implemented so that the development still delivers the fully functioning biodiversity objectives of the originally approved scheme. The approved plan will be implemented in accordance with the approved details.</p> <p>Reason: Biological communities are constantly changing and require positive management to maintain their conservation value. The implementation of a LEMP will ensure the long-term management of vegetated shingle habitat required to compensate for the loss of Black Rock Local Wildlife Site and to mitigate impacts on Volk's Railway LWS.</p> <p><b>Condition 10.</b> <u>With the exception of the Duke's Mound Works</u>, no development above ground floor slab level of any other part of the development site hereby permitted shall take place until a drainage strategy detailing the proposed means of foul water disposal and an implementation timetable, has been submitted to and approved in writing by, the Local Planning Authority in consultation with the sewerage undertaker. The development shall be carried out in accordance with the approved scheme and timetable.</p> <p>Reason: To ensure adequate foul sewage drainage/treatment is available prior to development commencing and to comply with policy SU5 of the Brighton &amp; Hove Local Plan.</p> <p><b>Condition 11.</b> (i). <u>With the exception of the Duke's Mound Works</u>, no works pursuant to this permission shall commence until there has been submitted to and approved in writing by the local planning authority:</p> <p>(a) <u>An intrusive site investigation and analysis report as per the recommendations</u></p>
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contained within the Geotechnical and Geoenvironmental Desk Study produced by Mott MacDonald, dated 31st January 2020, document reference: 409765-MMD-00-XX-RP-YG-0001 and in accordance with BS10175: 2011:2013 + A1 Guidelines for the Code of Practice for Contaminated Land and CLR Report No. 4 Sampling Strategies.

And if notified in writing by the local planning authority that the results of the site investigation are such that site remediation is required then,

(b) a detailed scheme for remedial works and measures to be undertaken to avoid risk from contaminants and/or gases when the site is developed and proposals for future maintenance and monitoring. Such a scheme shall include nomination of a competent person to oversee the implementation of the works.

(ii). The development hereby permitted shall not be occupied or brought into use until there has been submitted to, and approved in writing by, the local planning authority a written verification report by a competent person approved under the provisions of condition (i)b that any remediation scheme required and approved under the provisions of condition (i)b has been implemented fully in accordance with the approved details (unless varied with the written agreement of the local planning authority in advance of implementation). Unless otherwise agreed in writing by the local planning authority the verification report shall comprise:

- a) built drawings of the implemented scheme;
- b) photographs of the remediation works in progress;
- c) certificates demonstrating that imported and/or material left in situ is free from contamination.

Reason: To safeguard the health of future residents or occupiers of the site and to comply with policy SU11 of the Brighton & Hove Local Plan.

**Condition 13.** With the exception of the Duke's Mound Works, no development shall take place until a detailed design and associated management and maintenance plan of surface water drainage for the site using sustainable drainage methods as referred to in the submitted Design Access Statement (Jan 2020) has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved detailed

design prior to any demolition or construction commencing.  
Reason: To ensure that the principles of sustainable drainage are incorporated into this proposal and to comply with policy SU3 of the Brighton & Hove Local Plan

**Condition 15.** Notwithstanding the plans submitted, with the exception of the Duke's Mound Works, no development of the Black Rock site shall commence until a scheme detailing the design of the proposed highway works as illustrated within the planning application has been submitted to and approved in writing by the Local Planning Authority in consultation with the Highway Authority. The submitted scheme shall include full details of the following:

- Geometry and layout, including dimensions and visibility splays
- Pavement constructions and surfacing, kerbs and edge restraints
- Levels and gradients
- Lighting
- Drainage
- Street furniture
- Trees, other planting, growing media and planting aids
- Traffic signs and road markings;

The scheme shall include a completed a Road Safety Audit up to Stage 2 in accordance with the Highway Authority's standards at that time, with the Highway Authority acting as Overseeing Organisation. The Road Safety Audit Brief and Report, and all other road safety audit documents, shall be submitted with the scheme.

If the scheme proposes that any areas are shared between pedestrians and vehicles or where recommendations in Guidance On the Use of Tactile Paving Surfaces are not met then relevant disabled user groups (and/or national organisations representing them) shall be engaged as part of the design development process and the submitted scheme shall include both of the following.

- A Participative Inclusive Design Statement. This shall: explain the engagement undertaken with disabled user groups during the design development process; record their views and suggestions on the different options; and explain how these have shaped the submitted design proposals and other management

			<p>plans. Where it has not been considered possible to accommodate views and suggestions in the submitted proposals and plans then the reasons for this shall be detailed.</p> <ul style="list-style-type: none"> <li>An Equality Assessment. As a minimum this shall identify and explain: each adverse impact arising from the proposals for different protected character groups; how these are known (which may be from appropriate consultation/engagement, research or guidance relevant to the protected character groups impacted); the alternatives considered to avoid or minimise these impacts; and, where some residual adverse impact remain, the objective justifications for why complete avoidance is not considered possible and why the scheme should nonetheless be considered acceptable.</li> </ul> <p>The approved scheme shall be implemented prior to occupation of the Black Rock site and shall include the implementation of the recommendations of the Stage 3 Road Safety Audit, with the Highway Authority acting as Overseeing Organisation. Thereafter the approved scheme shall be retained.</p> <p>Reason: In the interest of highway safety, inclusivity, sustainability, quality design, the historic environment and public amenity and to comply with policies TR7, TR11, TR12, TR14, TR15, TR18, SU3, SU5, QD1, QD2, QD3, QD14, QD20, QD25, QD26, QD27 and HE6 of the Brighton &amp; Hove Local Plan and SA6, CP7, CP9, CP12, CP13 and CP15 of the Brighton &amp; Hove City Plan Part One, and National Planning Policy Framework paragraphs 108-110.</p> <p><b>Condition 16.</b> <u>With the exception of the Duke's Mound Works</u>, no development shall commence until full details of the retaining wall structures, including location (above or below the adopted (public) highway), land drainage from behind the wall, surface water drainage away from the highway, cross sections, depth of footings, retained height, thickness of wall, construction materials, method of construction and design drawings and calculations have been submitted to and agreed in writing by the Local Planning Authority in consultation with the Civil Engineering group within the Council which will require an Approval in Principal process to be completed.</p> <p>Reason: To ensure the stability and safety of the adjacent pavement and to comply with Policy TR7 of the Brighton &amp; Hove Local Plan and SA6, CP7, CP9, CP12, CP13</p>
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			<p>and CP15 of the City Plan Part One.</p> <p><b>Informative 3.</b>  The lighting installation should comply with the recommendations of the Institution of Lighting Professionals (ILP) e.g. Guidance On Undertaking Environmental Lighting Impact Assessments. Please provide a report, and certification on completion, from a competent person to show the lighting installation complies with the guidance. These details need to include the predictions of both horizontal illuminance across the site and vertical illuminance affecting immediately adjacent receptors. External lighting for the development should be designed and positioned to:</p> <ul style="list-style-type: none"> <li>o Be the minimum required to perform the relevant lighting task.</li> <li>o Minimise light spillage and pollution.</li> <li>o Include landscaping/screening measures to screen illuminated areas in environmentally sensitive areas.</li> <li>o Avoid dazzle or distraction to drivers on nearby highways.</li> </ul> <p>Any external lighting designs must have reference to both horizontal and vertical illuminance to account for the varied sensitive receptors on and around the site.</p>
B	Avon Court, 12 Dallington Road	BH2020/00325	<p><b>Additional Condition</b>  3 swift boxes shall be placed on the external walls of the development hereby approved and shall be retained thereafter.</p> <p><b>Reason:</b> To enhance the biodiversity of the site and to comply with Policy CP10 of the Brighton &amp; Hove City Plan Part One and Supplementary Planning Document SPD11 Nature Conservation and Development.</p> <p><b>Additional Informative</b>  Swift boxes can be placed on any elevation, but ideally under shade-casting eaves. They should be installed in groups of at least three, at a height above 5m height, and preferably with a 5m clearance between the host building and other buildings or obstructions. Where possible avoid siting them above windows or doors.</p>
C	Varndean College, Surrenden Road	BH2020/00947	<p><b>Additional condition:</b>  The temporary classroom building 48-50 hereby permitted shall be permanently</p>

			<p>removed from the site on or before 19 July 2024, or when it is no longer required, whichever is the earlier. The land shall be restored to its condition immediately prior to the building being situated on the land within two months of removal of the building in accordance with a scheme of work to be submitted and approved in writing by the Local Planning Authority.</p> <p><b>Reason:</b> The structure is not considered suitable as a permanent form of development and permission is therefore granted for a temporary period only to comply with policies CP12, CP16 and CP17 of the Brighton &amp; Hove City Plan Part One.</p>																																								
D	105 Woodland Drive	BH2019/00694	<p>The submission of a full Arboricultural Assessment &amp; Outline Method Statement shows that 2 individual trees and a section of hedge would be removed from the site to facilitate the proposed new dwelling rather than 3 trees stated in the committee report.</p> <p>As a result of the full assessment conditions 1 and 10 are amended as follows and an additional condition regarding tree protection is added,</p> <p><b>Condition 1:</b> The development hereby permitted shall be carried out in accordance with the approved drawings listed below.</p> <p><b>Reason:</b> For the avoidance of doubt and in the interests of proper planning.</p> <table border="1"> <thead> <tr> <th>Plan Type</th><th>Reference</th><th>Version</th><th>Date Received</th></tr> </thead> <tbody> <tr> <td>Proposed Drawing</td><td>1581 - FE - 001</td><td>A</td><td>16 April 2020</td></tr> <tr> <td>Proposed Drawing</td><td>1581- FE - 002</td><td>A</td><td>16 April 2020</td></tr> <tr> <td>Proposed Drawing</td><td>1581 - FE - 003</td><td>A</td><td>16 April 2020</td></tr> <tr> <td>Proposed Drawing</td><td>1581 - FE - 004</td><td>A</td><td>22 April 2020</td></tr> <tr> <td>Proposed Drawing</td><td>1581 - FE - 005</td><td></td><td>22 April 2020</td></tr> <tr> <td>Proposed Drawing</td><td>1581- FE - 007</td><td></td><td>19 May 2020</td></tr> <tr> <td>Location and block plan</td><td>1581 - FE - 006</td><td></td><td>14 May 2020</td></tr> <tr> <td>Location and block plan</td><td>1581 - PA 001</td><td></td><td>8 March 2019</td></tr> <tr> <td>Other</td><td>NJCL</td><td></td><td>27 May 2020</td></tr> </tbody> </table>	Plan Type	Reference	Version	Date Received	Proposed Drawing	1581 - FE - 001	A	16 April 2020	Proposed Drawing	1581- FE - 002	A	16 April 2020	Proposed Drawing	1581 - FE - 003	A	16 April 2020	Proposed Drawing	1581 - FE - 004	A	22 April 2020	Proposed Drawing	1581 - FE - 005		22 April 2020	Proposed Drawing	1581- FE - 007		19 May 2020	Location and block plan	1581 - FE - 006		14 May 2020	Location and block plan	1581 - PA 001		8 March 2019	Other	NJCL		27 May 2020
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Location and block plan	1581 - PA 001		8 March 2019																																								
Other	NJCL		27 May 2020																																								

			615_02_230520		
			<p><b>Condition 10:</b>  No tree shown as retained on approved drawing NJCL 615_02_230520, received on the 27th May 2020, shall be cut down, uprooted, destroyed, or damaged in any manner during the development phase and thereafter within 5 years from the date of occupation of the building for its permitted use, other than in accordance with the approved plans and particulars or as may be permitted by prior approval in writing from the Local Planning Authority. Any trees or plants which within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species.</p> <p><b>Reason:</b> To enhance the appearance of the development in the interest of the visual amenities of the area, to provide ecological, environmental and bio-diversity benefits and to maximise the quality and usability of open spaces within the development in compliance with policies QD15 of the Brighton &amp; Hove Local Plan and CP12 and CP13 of the Brighton &amp; Hove City Plan Part One.</p> <p><b>Additional Conditions</b>  The development hereby permitted shall not be commenced (including demolition and all preparatory work) until the protection measures identified in the submitted Arboricultural Assessment &amp; Outline Method Statement received on the 27<sup>th</sup> May 2020 are in place and retained throughout the construction process. The fences shall be erected in accordance with British Standard BS5837 (2012) Trees in relation to design, demolition and construction – Recommendations and shall be retained until the completion of the development and no vehicles, plant or materials shall be driven or placed within the areas enclosed by such fences.</p> <p><b>Reason:</b> As this matter is fundamental to protecting the trees which are to be retained on the site during construction works in the interest of the visual amenities of the area and to comply with policies QD16 of the Brighton &amp; Hove Local Plan and CP12 of the Brighton &amp; Hove City Plan Part One and SPD06: Trees and Development Sites.</p>		



			<p>3 swift bricks / boxes shall be incorporated within the external walls of the development hereby approved and shall be retained thereafter.</p> <p><b>Reason:</b> To enhance the biodiversity of the site and to comply with Policy CP10 of the Brighton &amp; Hove City Plan Part One and Supplementary Planning Document SPD11 Nature Conservation and Development.</p> <p><b>Additional Informative</b>  Swift bricks/boxes can be placed on any elevation, but ideally under shade-casting eaves. They should be installed in groups of at least three, at a height above 5m height, and preferably with a 5m clearance between the host building and other buildings or obstructions. Where possible avoid siting them above windows or doors. Swift bricks should be used unless these are not practical due to the nature of construction, in which case alternative designs of suitable swift boxes should be provided in their place.</p>
G	Hove Park Nevill Campus, 38 Nevill Road	BH2020/00206	<p><b>Additional Condition:</b> Three (3) swift bricks/boxes shall be incorporated within the external walls of the development hereby approved and shall be retained thereafter.</p> <p><b>Reason:</b> To enhance the biodiversity of the site and to comply with Policy CP10 of the Brighton &amp; Hove City Plan Part One and Supplementary Planning Document SPD11 Nature Conservation and Development.</p> <p><b>Additional Informative:</b> Swift bricks/boxes can be placed on any elevation, but ideally under shade-casting eaves. They should be installed in groups of at least three, at a height above 5m height, and preferably with a 5m clearance between the host building and other buildings or obstructions. Where possible avoid siting them above windows or doors. Swift bricks should be used unless these are not practical due to the nature of construction, in which case alternative designs of suitable swift boxes should be provided in their place.</p>

I	6 Princes Crescent, Hove	BH2020/00724	<p>Recently further information has been submitted that addresses the matters for which pre-commencement conditions were included, therefore the two conditions shall be amended to secure compliance with the received information instead.</p> <p>In addition to the biodiversity improvements already included in the proposal, a condition securing three swift bricks within the design of the dwellinghouse is considered necessary.</p> <p><b>Vary Condition 12:</b> The works of demolition/dismantlement hereby permitted shall be carried out in accordance with the Contract submitted to the Local Planning Authority on 27<sup>th</sup> May 2020 to ensure that building work on the site the subject of this consent is commenced within a period of 6 months following commencement of demolition/dismantlement in accordance with a scheme for which planning permission has been granted. Reason: As this matter is fundamental to the acceptable delivery of the permission to prevent premature demolition in the interests of the character and appearance of the Conservation Area and to comply with policy HE8 of the Brighton &amp; Hove Local Plan and CP15 of the Brighton &amp; Hove City Plan Part One.</p> <p><b>Vary Condition 19:</b> The works of demolition/dismantlement hereby permitted shall be carried out in accordance with the measures detailed in the letter from ProSafe Consultants submitted on 27<sup>th</sup> May 2020 unless otherwise agreed in writing by the Local Planning Authority. Reason: To reduce the demand for new building materials; and to comply with policies SA6 and CP8 of the City Plan Part One</p> <p><b>Additional Condition:</b> Three (3) swift bricks/boxes shall be incorporated within the external walls of the development hereby approved and shall be retained thereafter. <b>Reason:</b> To enhance the biodiversity of the site and to comply with Policy CP10 of the Brighton &amp; Hove City Plan Part One and Supplementary Planning Document SPD11 Nature Conservation and Development.</p>
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			<p><b>Additional Informative:</b> Swift bricks/boxes can be placed on any elevation, but ideally under shade-casting eaves. They should be installed in groups of at least three, at a height above 5m height, and preferably with a 5m clearance between the host building and other buildings or obstructions. Where possible avoid siting them above windows or doors. Swift bricks should be used unless these are not practical due to the nature of construction, in which case alternative designs of suitable swift boxes should be provided in their place.</p>
J	Flat at 39 Guildford Road	BH2020/00235	<p>The following text is added to the officer report:</p> <ol style="list-style-type: none"> <li>1. In application header add 'West Hill' to Con Area box</li> <li>2. Following section 2.1 insert 'The property is situated within the West Hill Conservation Area.'</li> <li>3. Between sections 8.6 and 8.7 insert: Design and Appearance: The application property is situated within the West Hill Conservation Area. When considering whether to grant planning permission for development in a conservation area the council has a statutory duty to pay special attention to the desirability of preserving or enhancing the character or appearance of the area. Case law has held that the desirability of preserving the character or appearance of a conservation area must be given "considerable importance and weight"</li> </ol> <p>The application has been amended during consideration to remove all proposed external works relating to the roof. No other external works are proposed which would require planning permission.</p>
K	47 Eley Drive	BH2020/00791	<p>Additional neighbour representation received <b>objecting</b> to the scheme.</p> <p>Comment: The impact of the development on neighbouring amenity has been assessed in the report.</p>



# 105 Woodland Drive

**BH2019/00694**



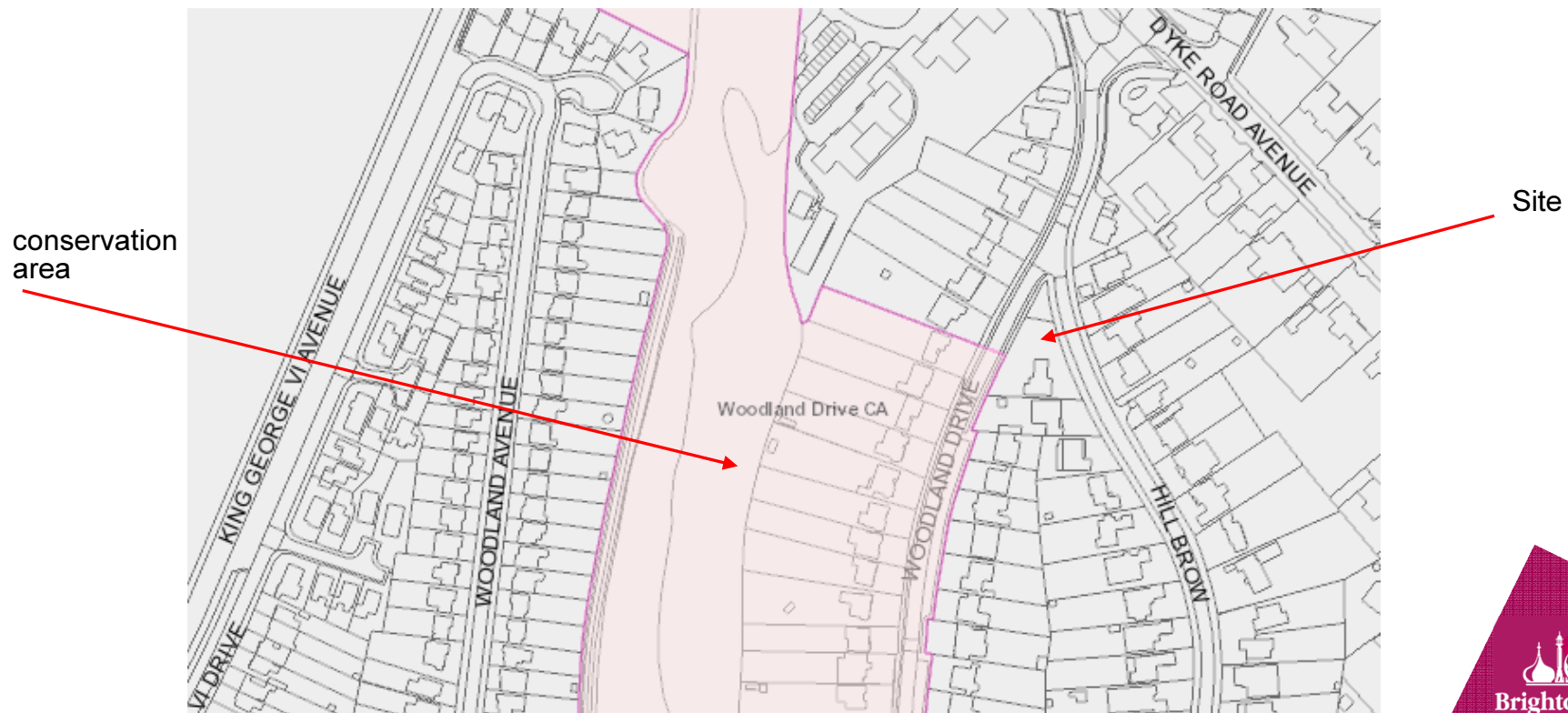
**Brighton & Hove  
City Council**

# Application Description

- Planning permission is sought for;

Erection of a three storey, 4no. bedroom house, incorporating the demolition of part of the existing double garage to provide a single garage for the existing house, revisions to boundary wall, parking and associated works

# Map of application site



# Existing Location Plan



**+SP** Existing site plan - scale 1:1250 at A1  
Scale in Metres  
10 20 30 40 50 100

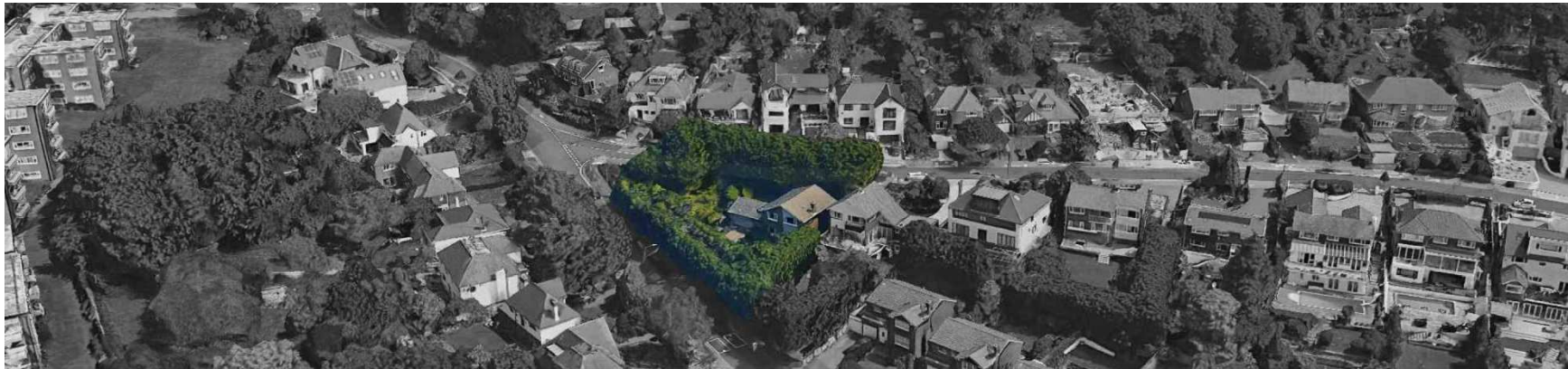


1581-PA-001 - 006



# Existing Visual(s)

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1581-PA-001 - 006

# Aerial photo(s) of site



# 3D Aerial photo of site

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# Existing Front Elevation of 105 Woodland Drive



# **Existing Rear Elevation of 105 Woodland Drive**



# **Existing Side Elevation of 105 Woodland Drive**

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# **Existing View from Woodland Drive Near Junction with Hill Brow**

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# Existing View from Hill Brow

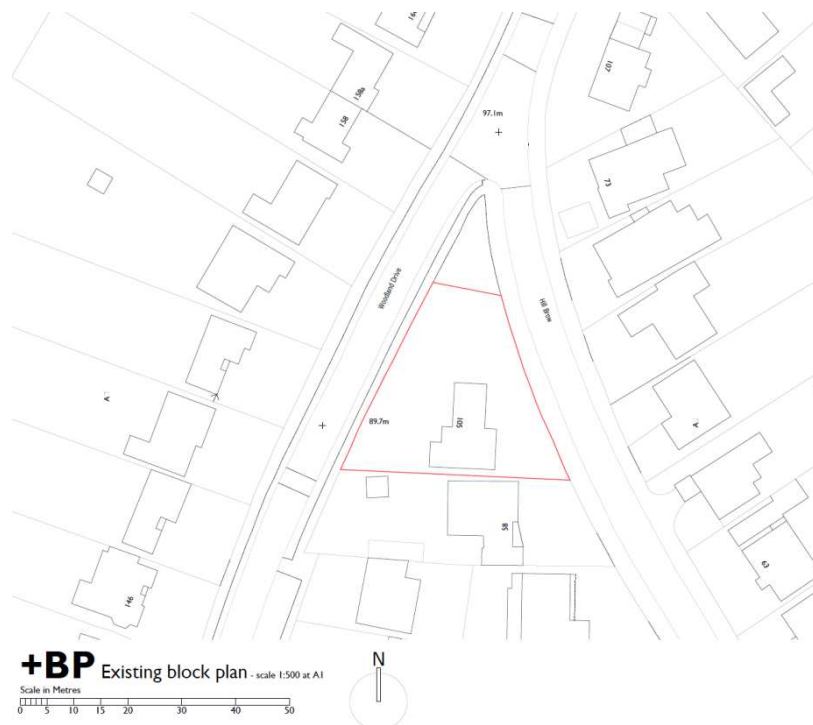
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# Existing Block Plan

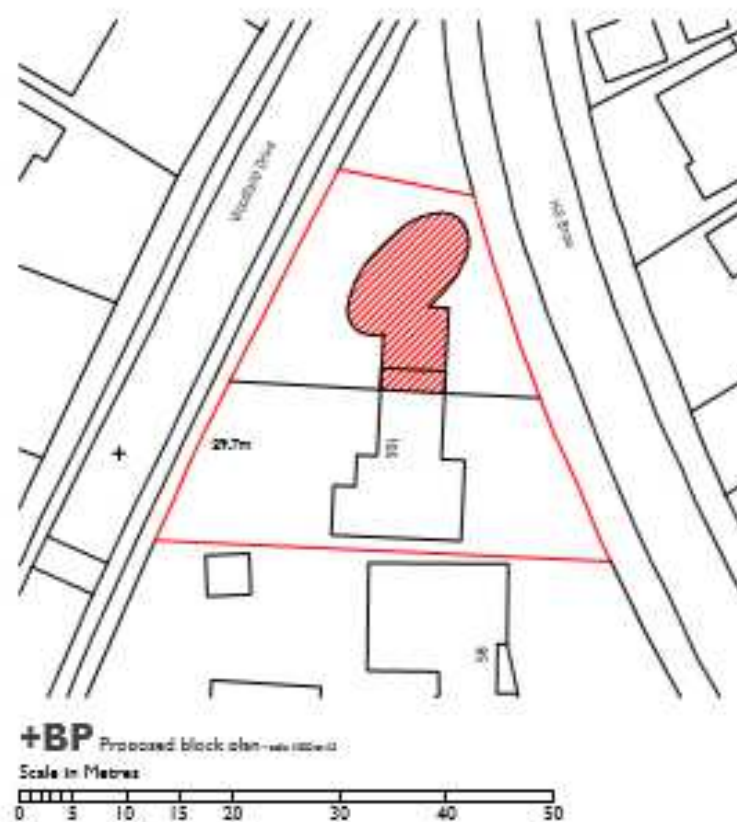
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1581-PA-001 - 006



# Proposed Block Plan



# Proposed Elevation/Site Section

Interior of the site facing Woodland Drive



1581-FE-004 REV A

# Proposed Elevation/Site Section

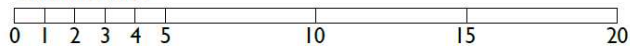
Interior of the site looking south

Previous proposed scheme



**+SX** Proposed section BB - scale 1:200 at A3

Scale in Metres



1581-FE-004 REV A

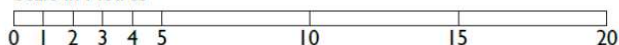
# Proposed Elevation/Site Section

Interior of the site facing Hill Brow



**+SX** Proposed section CC - scale 1:200 at A3

Scale in Metres



1581-FE-004 REV A

# Proposed Elevation/Site Section

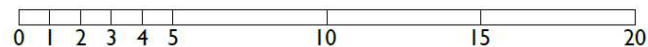
Interior of the site looking north



Previous proposed scheme

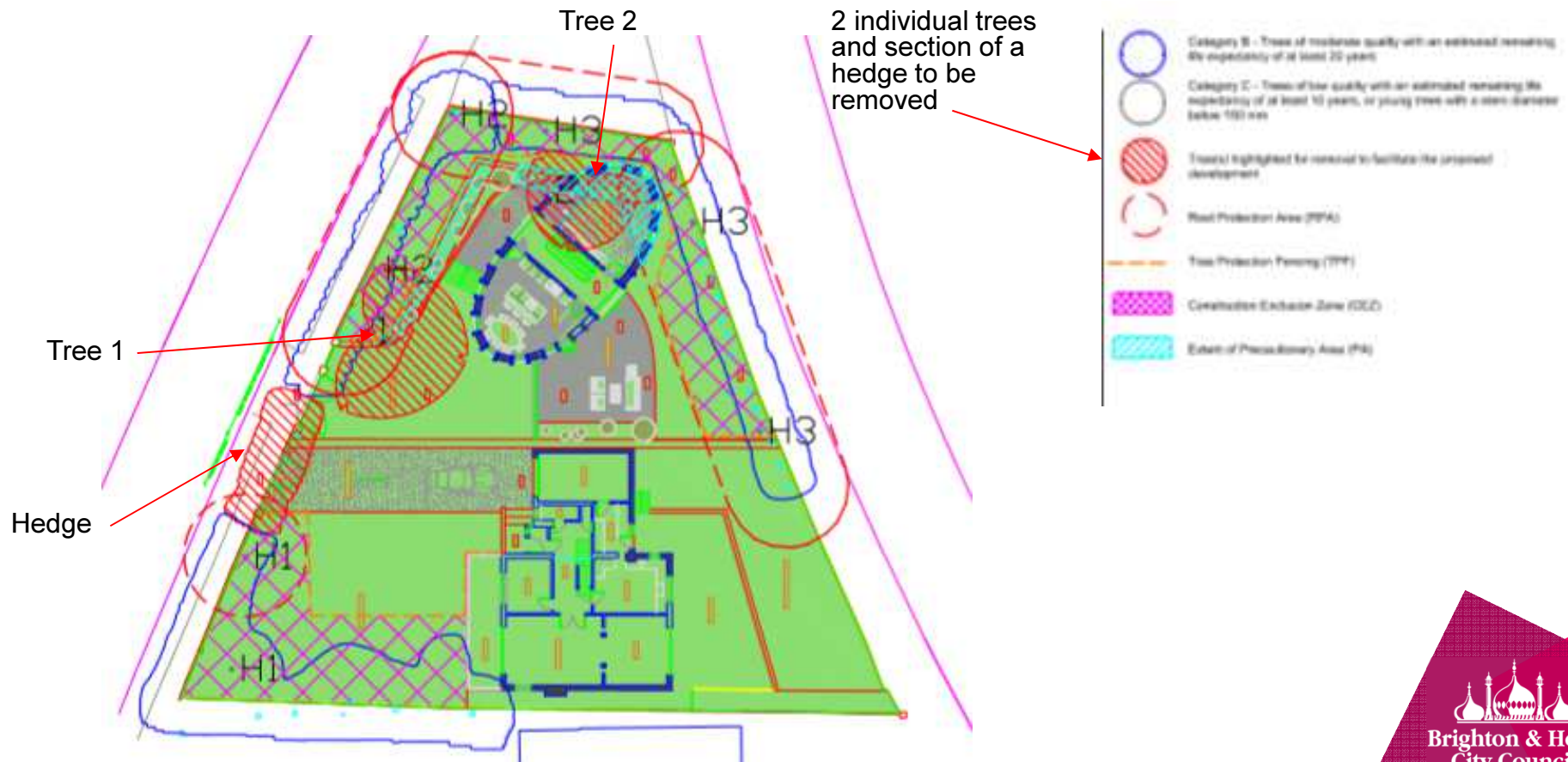
**+SX** Proposed section DD - scale 1/200 at A3

Scale in Metres



1581-FE-004 REV A

# Tree Plan





# Proposed visuals Woodland Drive

(view east)

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# Proposed visuals Woodland Drive

(view south)

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# Proposed visuals Hill Brow (view west)

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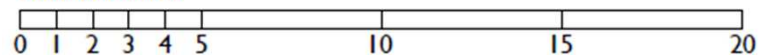


# Proposed west elevation (illustrated without trees)



**+EL** Proposed west elevation (illustrated without trees) - scale 1:200 at A3

Scale in Metres

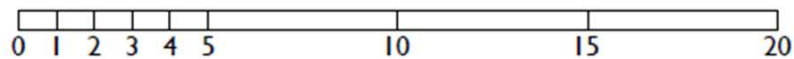


# Proposed north elevation (illustrated without trees)



**+EL** Proposed north elevation (illustrated without trees) - scale 1:200 at A3

Scale in Metres

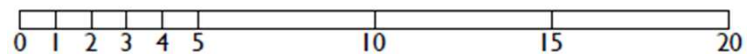


# Proposed east elevation (illustrated without trees)



**+EL** Proposed east elevation (illustrated without trees) - scale 1:200 at A3

Scale in Metres

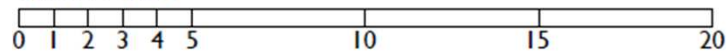


# Proposed south elevation (illustrated without trees)



**+EL** Proposed south elevation (illustrated without trees) - scale 1:200 at A3

Scale in Metres



# Key Considerations in the Application

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- Principle of development
- Design, appearance & impact on setting of Conservation Area
- Impact on the amenity of occupants of adjacent properties
- Highways and transport considerations
- Standard of accommodation
- Arboriculture
- Ecology
- Sustainability



# Conclusion and Planning Balance

- Development results in the provision of an additional family dwelling
- Whilst proposal results in the loss of 2 trees and part of a hedge, it is considered that the proposal is an effective use of an existing garden area
- Design, height, bulk and scale considered acceptable
- Following amendments to height and scale, proposal not considered visually harmful to wider area especially setting of adjacent conservation area
- Good standard of accommodation provided for future occupiers.
- No significant adverse harm to amenity of neighbouring properties or gardens
- No arboriculture, ecology or highway objections

The application is therefore recommended for **approval**



# 29 Woodbourne Avenue

**BH2020/00187**



**Brighton & Hove  
City Council**

# Application Description

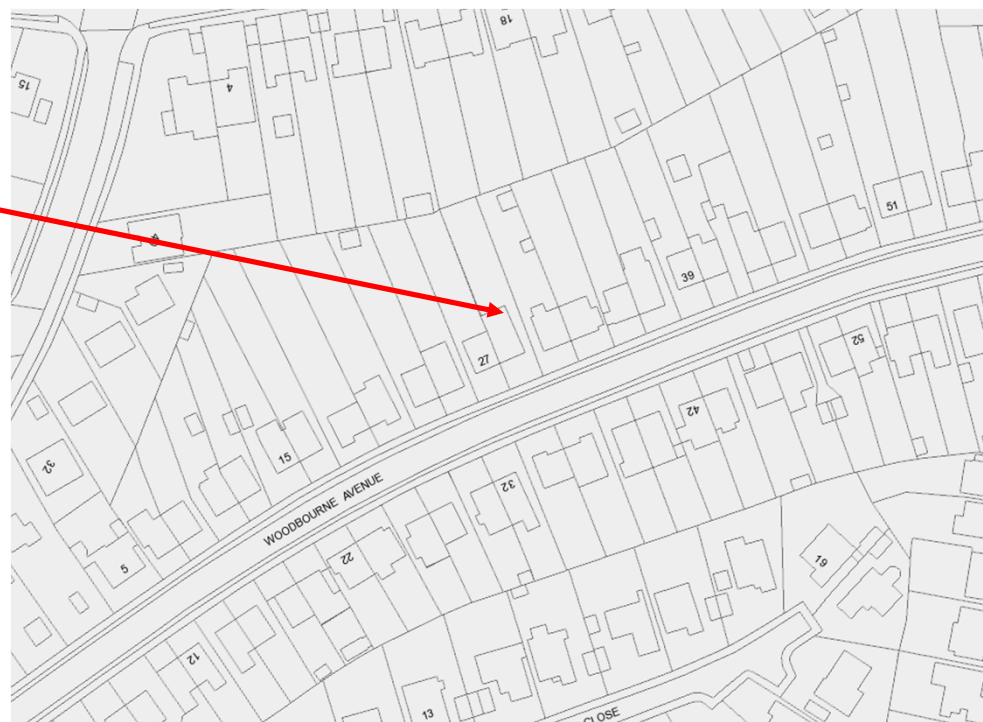
- Permission is sought for;

Variation of condition 2 of application  
BH2018/03661 (Retrospective consent for rear  
dormer extension) to remove requirement to  
replace cladding with tile hanging to the side  
gable.

# Map of application site

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Site



# Existing Block Plan

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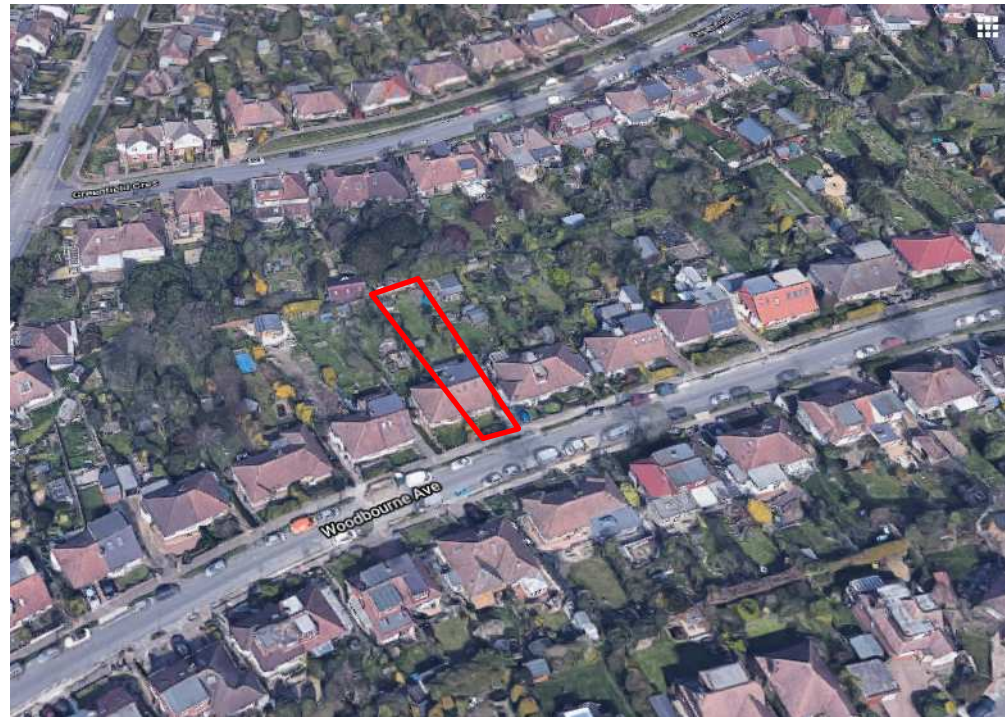
# Aerial photo(s) of site

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# 3D Aerial photo of site

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# Pre-Existing Front Elevation (July 2012)

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Site

# Pre-Existing Side Elevation (July 2012)

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Site



# Existing Front Elevation (February 2020)

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Brighton & Hove  
City Council

ID

# Existing Side Elevation (February 2020)

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# Existing Rear Elevation (February 2020)

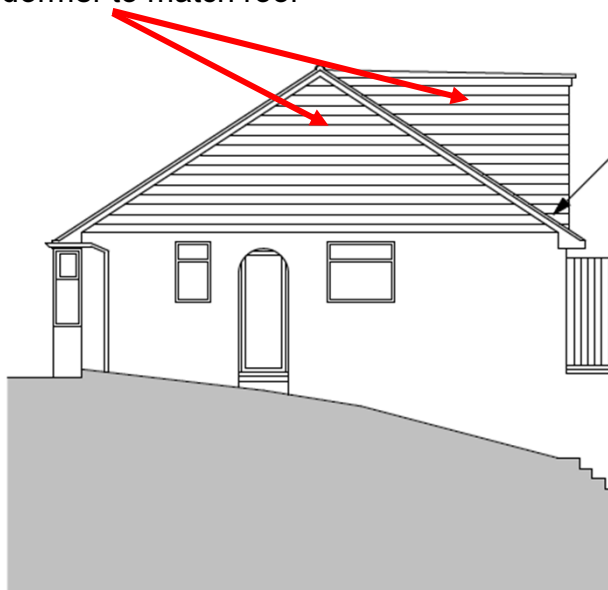
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Under application BH2018/03661  
cladding to dormer shall be replaced  
with tile hanging

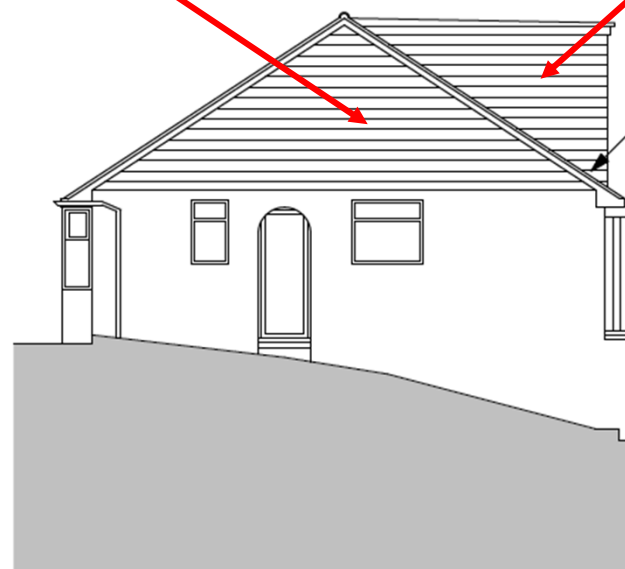
# Side (East) Elevation - Approved and Proposed

Tile hanging to side gable and  
rear dormer to match roof



Approved design  
under BH2018/03661

Cladding to side gable

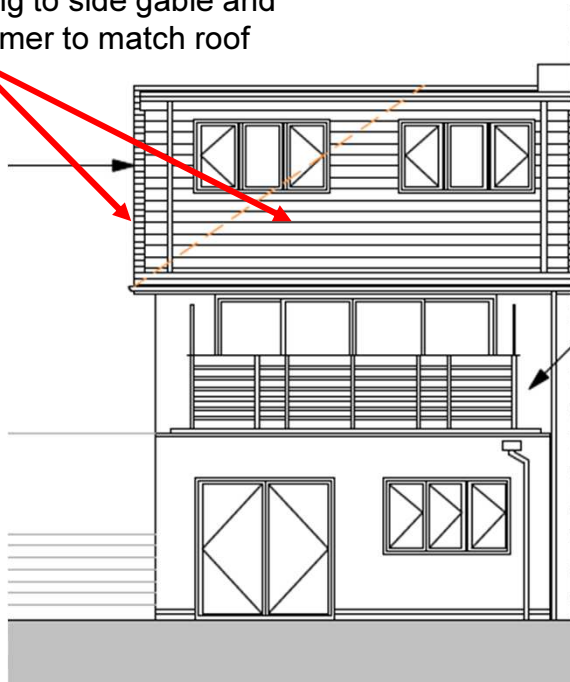


Tile hanging to dormer  
(as approved - no change)

Proposed

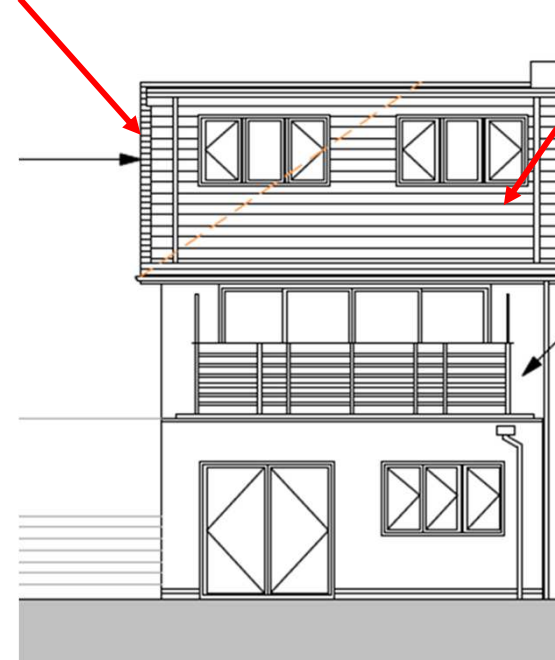
# Rear Elevation - Approved and Proposed

Tile hanging to side gable and rear dormer to match roof



Approved design under  
BH2018/03661

Cladding to side gable



Proposed

Tile hanging to dormer  
(as approved – no change)

# Street Scene Photographs

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Number 23 Woodbourne Avenue



Number 25 Woodbourne Avenue



# Street Scene Photographs

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Cladding to side gable  
and rear dormer

Number 61 Woodbourne Avenue

# Key Considerations in the Application

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- The impact of the cladding to the gable end on the host property and wider street scene.



# Conclusion and Planning Balance

- The retention of cladding to the side gable would appear incongruous and relates poorly to the tiled roof of the main dwelling.
- The resultant appearance is considered to cause harm to the host property and wider area.
- As such the development is recommended for refusal.



# Hove Park Nevill Campus, 38 Nevill Road

**BH2020/00206**

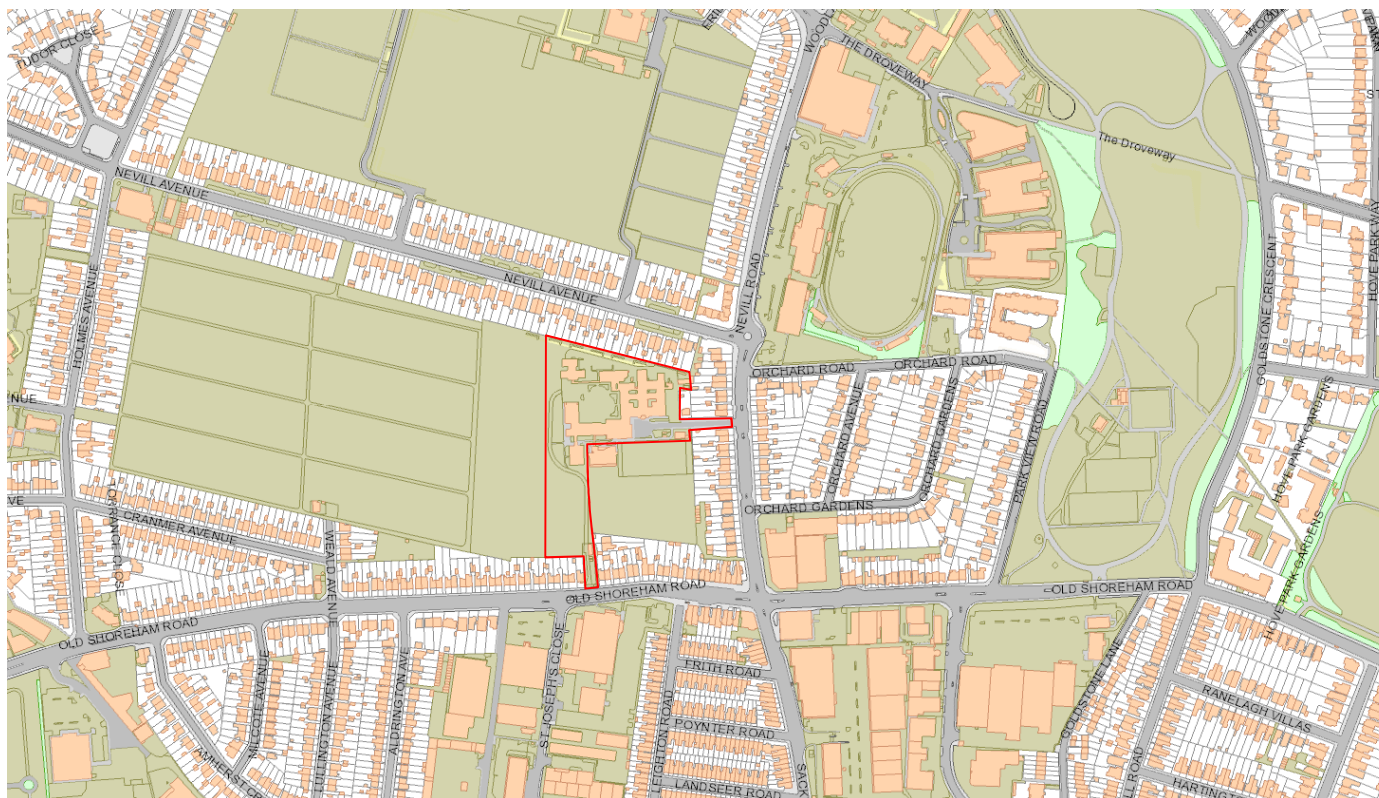


**Brighton & Hove  
City Council**

# **Application Description**

Erection of double height extension to existing gymnasium to create a two court sports hall incorporating part demolition of existing gymnasium & corridor, replacement of existing doorway with window, two new access ramps and refurbishment works.

# Map of application site

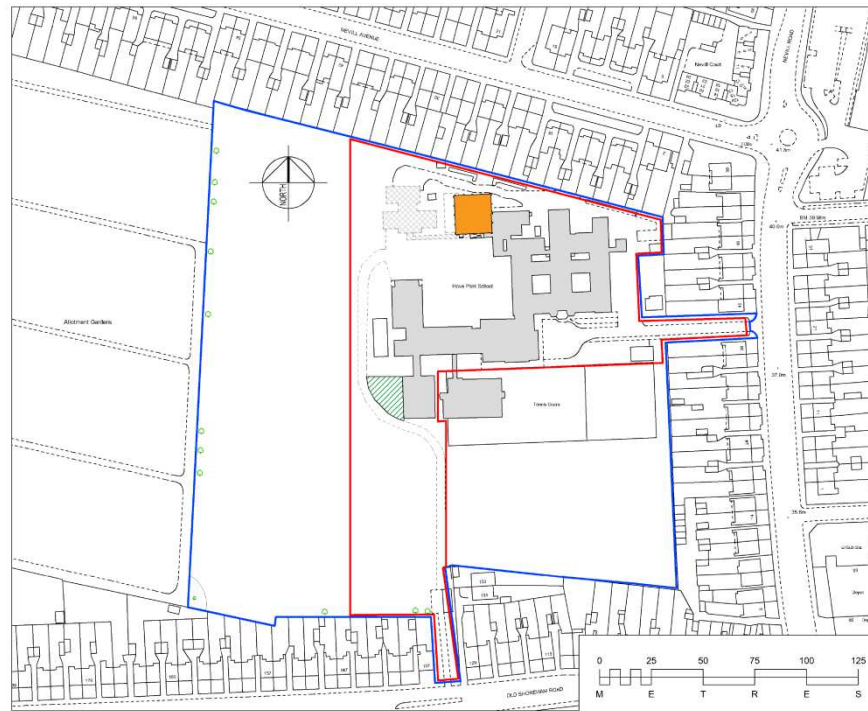


# Existing Location Plan



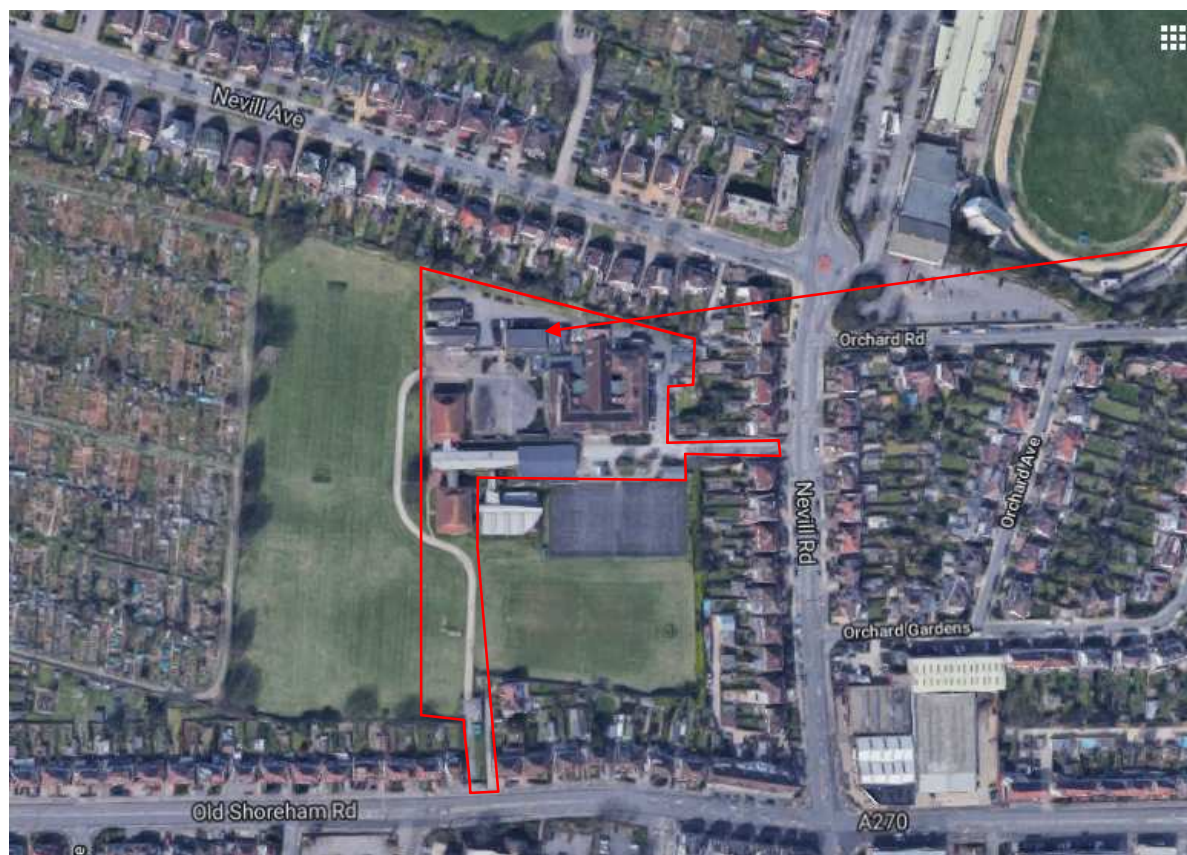
NC-001

# Proposed Location Plan





# Aerial photo(s) of site



Existing gymnasium



# 3D Aerial photo of site from north



Existing gymnasium



Brighton & Hove  
City Council



# 3D Aerial photo of site from south

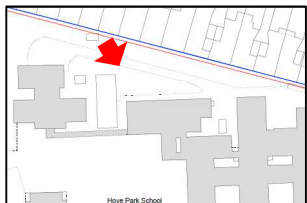
Existing gymnasium



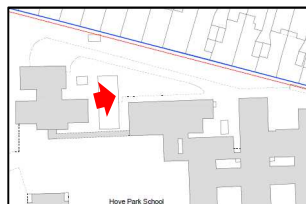
Brighton & Hove  
City Council

# Photo(s) of site

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# Photo(s) of site



Closest Nevill Avenue  
properties – nos. 16,18,20

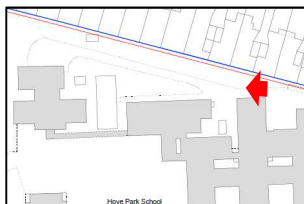
Existing gymnasium



Brighton & Hove  
City Council



# Photo(s) of site

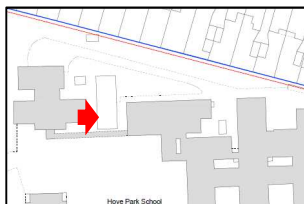


Existing dining hall (to be demolished - BH2019/01463)



Area to be landscaped to widen road

# Photo(s) of site



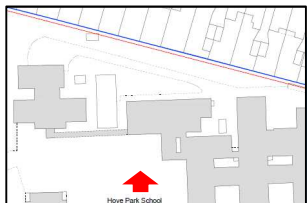
Corridor to be demolished with new access ramp



Brighton & Hove  
City Council

# Photo(s) of site

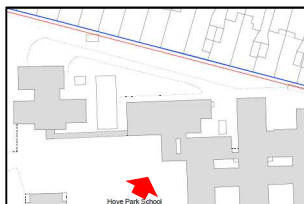
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Brighton & Hove  
City Council



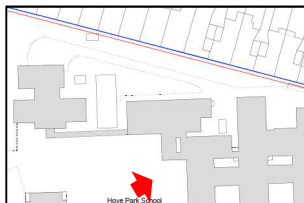
# Photo(s) of site



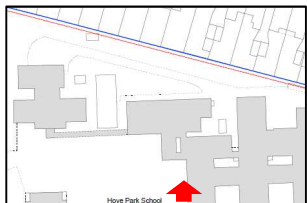


# Photo(s) of site

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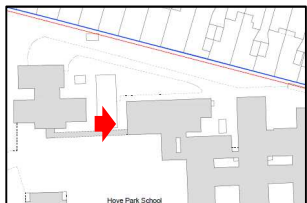


# Photo(s) of site



New access ramp

# Photo(s) of site



Corridor to be demolished with new access ramp

# Proposed Block Plan

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NC-004

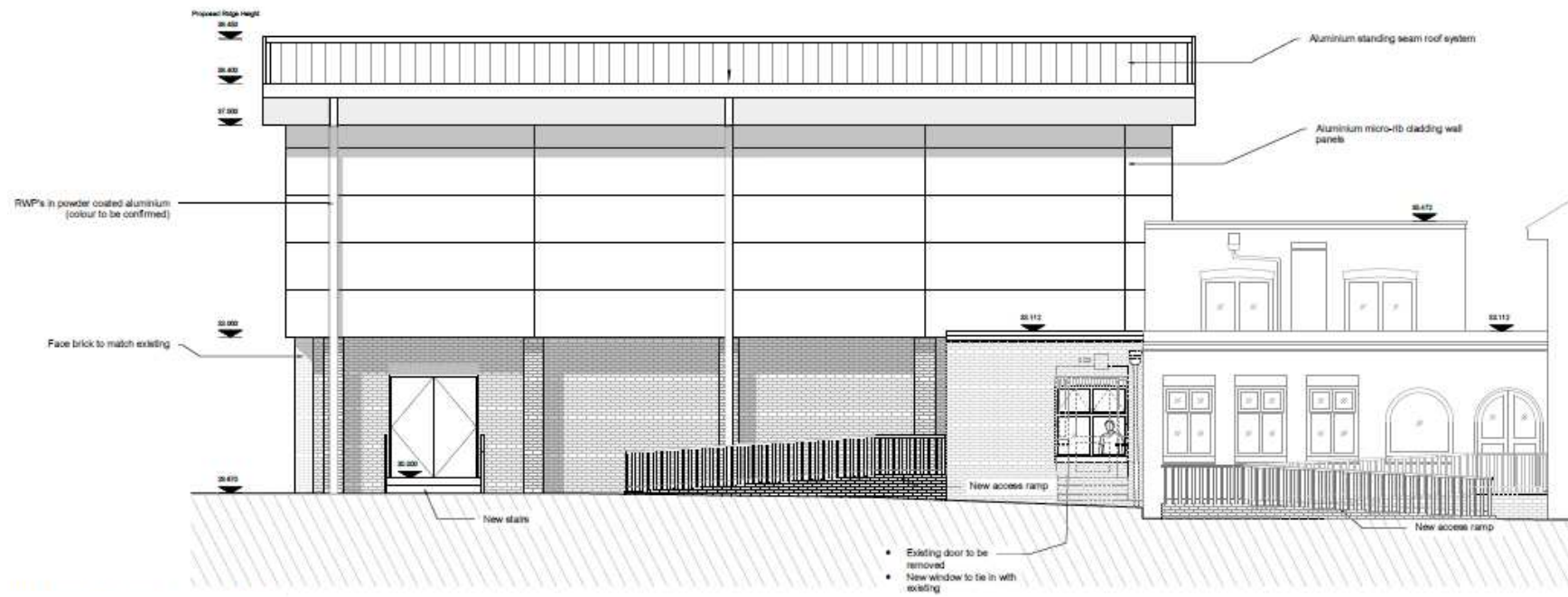
# Proposed North Elevation



NC-012-1

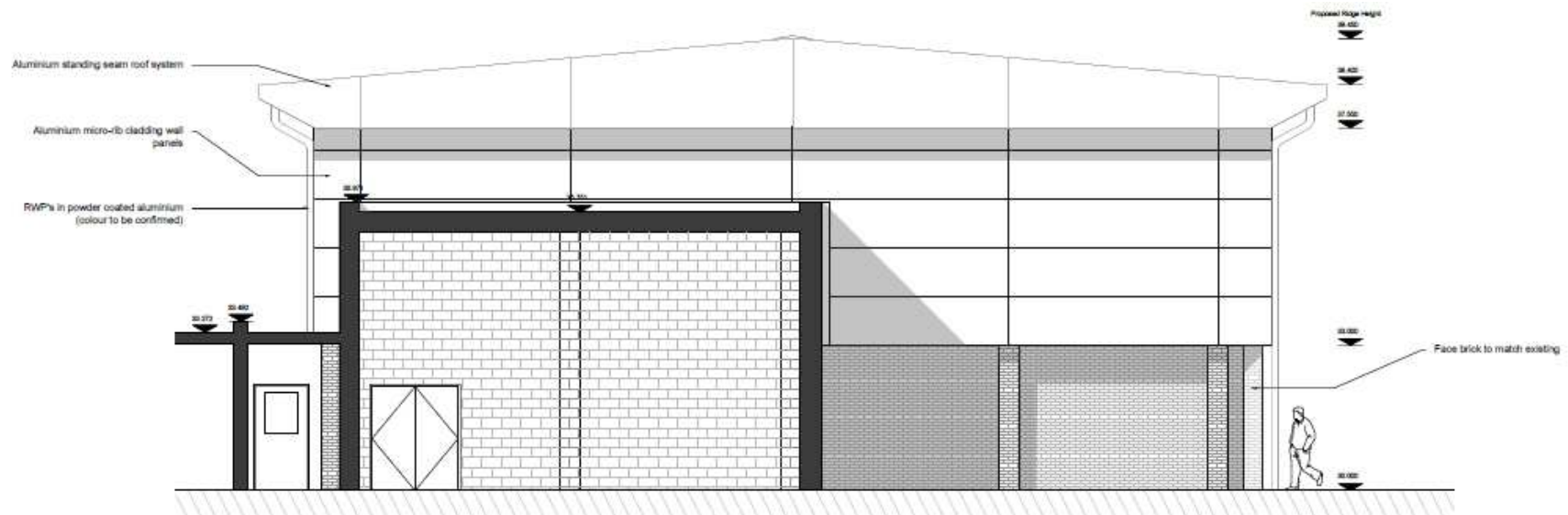


# Proposed South Elevation



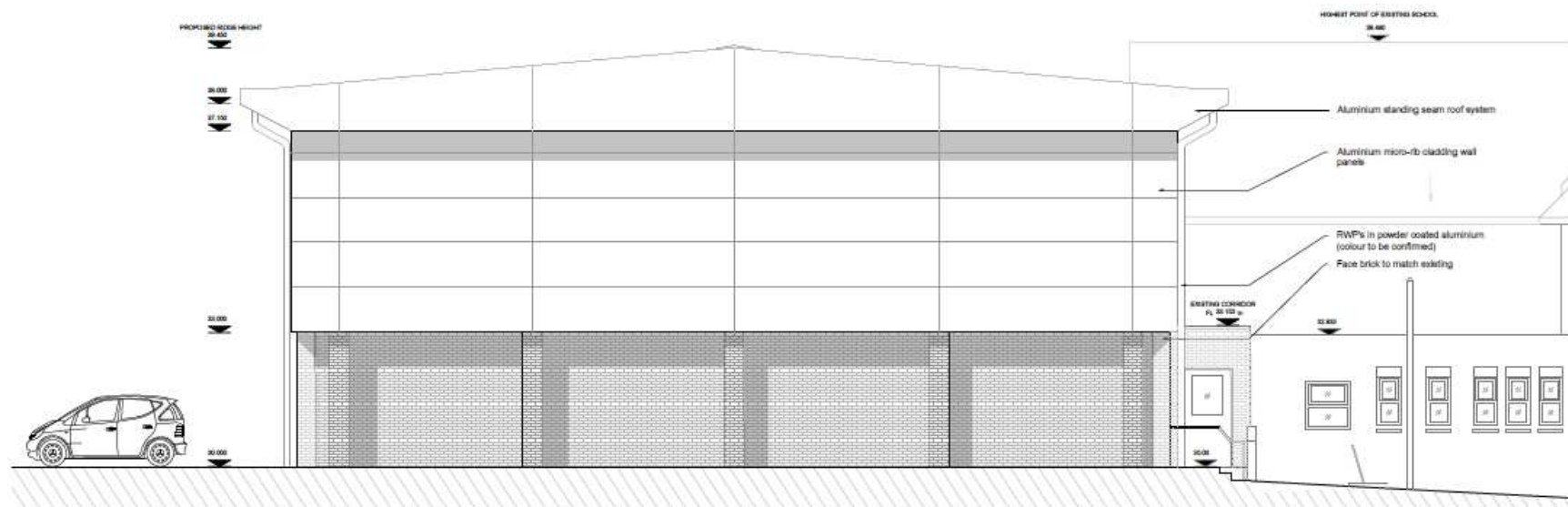
NC-012-2

# Proposed East Elevation



Brighton & Hove  
City Council

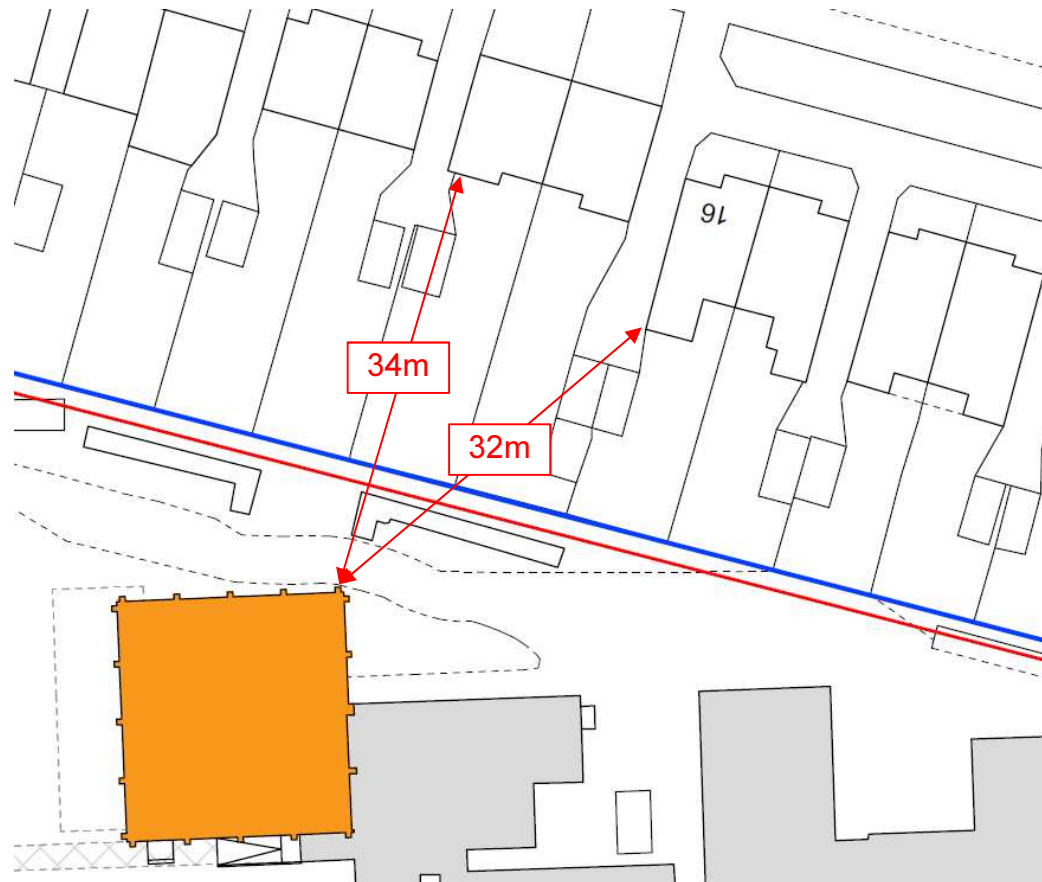
# Proposed West Elevation



Brighton & Hove  
City Council



# Spacing from Nevill Avenue properties



# Key Considerations in the Application

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- Principle of development
- Design & appearance
- Impact on amenity
- Archaeology
- Ecology
- Sustainable transport

# Conclusion and Planning Balance

---

- The proposal would enhance the provision of educational facilities on site and would accord with policies CP16, CP18 and CP18 of the Brighton and Hove City Plan Part One;
  - No increase in 'external lets' is proposed with the sports hall being ancillary to the main D1 Education school use;
  - The sports hall would be a large building, however no concerns are held regarding design & appearance;
  - There would likely be some overshadowing of the residential gardens/properties to the north. This would be most noticeable in wintertime at dawn and dusk;
  - Concerns regarding Archaeology, Ecology and waste generated during construction/demolition can be overcome through the recommended pre-commencement conditions;
  - There would be no increase in trip generation as there would be no increase in pupil numbers or 'external lets'.
- 
- On balance, the benefits of the scheme in the provision of enhanced educational facilities are considered to outweigh the potential overshadowing of properties to the north/north-east. Approval is therefore recommended, subject to conditions.



Brighton & Hove  
City Council



# Flat at 39 Guildford Road

**BH2020/00235**



**Brighton & Hove  
City Council**

# Application Description

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- Proposal is to convert a 3 bedroom maisonette to a 4 bedroom small HMO (C4 Use Class)
- No external alterations are proposed

# Location Plan

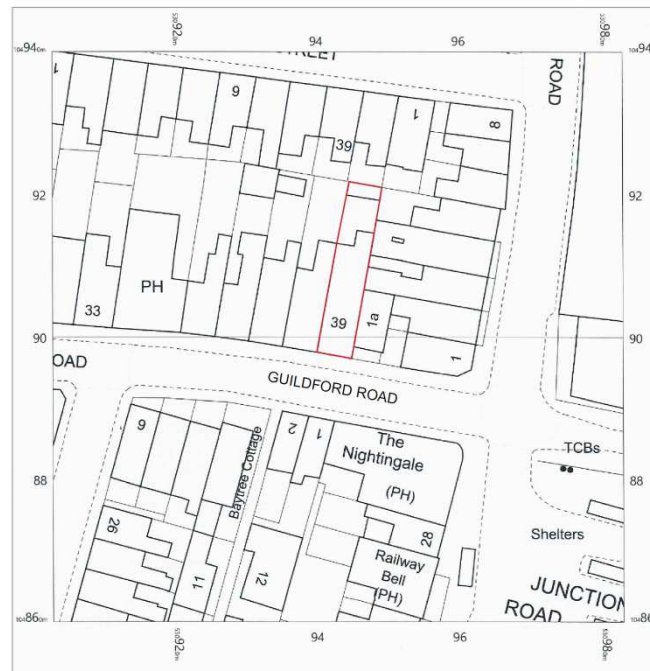


**LOCATION PLAN**  
**SCALE 1:1250**



A-01

# Block Plan



**BLOCK PLAN**  
SCALE 1:500



A-01



# Aerial photo(s) of site



A-01

# 3D Aerial photo of site

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# Street photo of site

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# Photo without scaffolding

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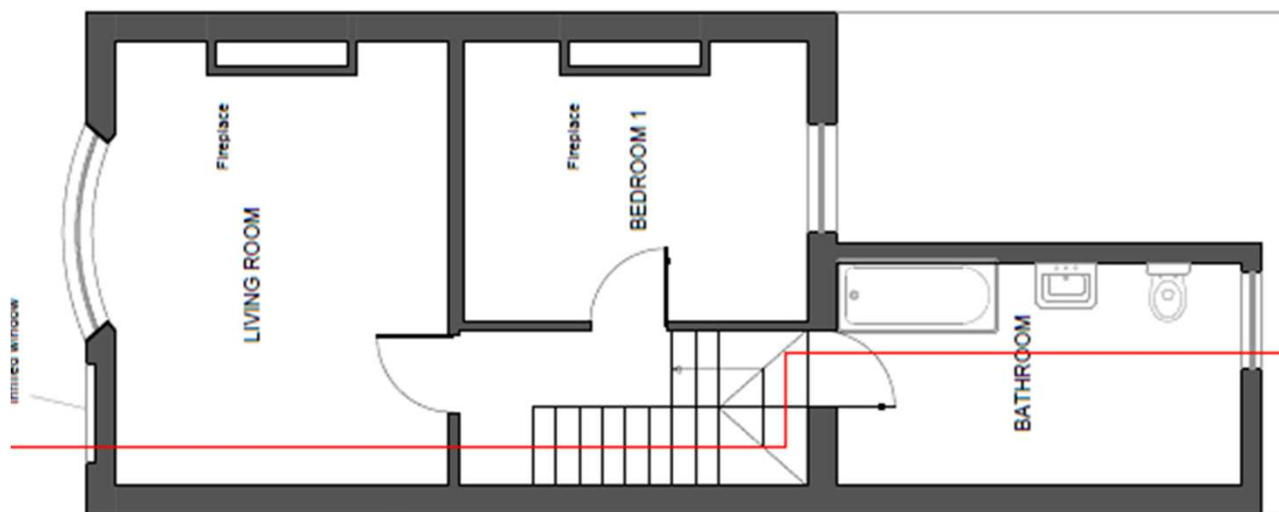


# Rear of property



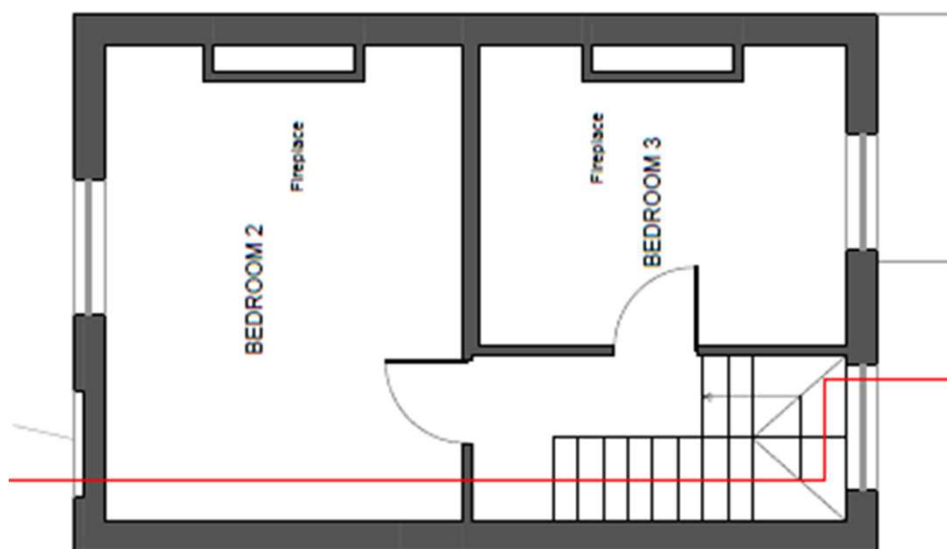
# Existing first floor plan

---

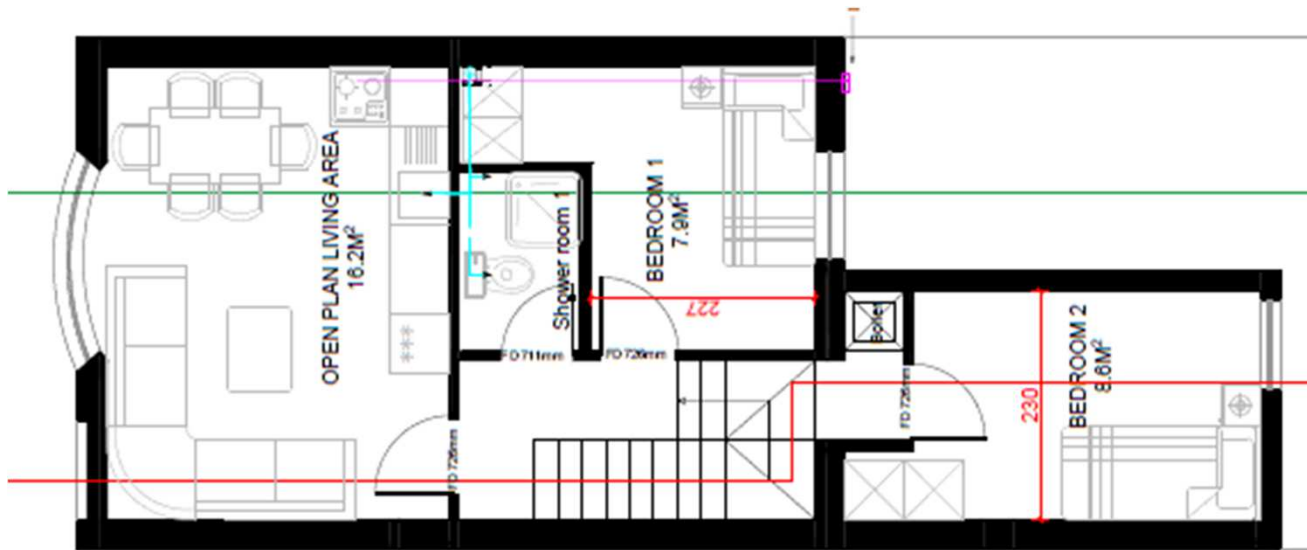


# Existing second floor plan

---

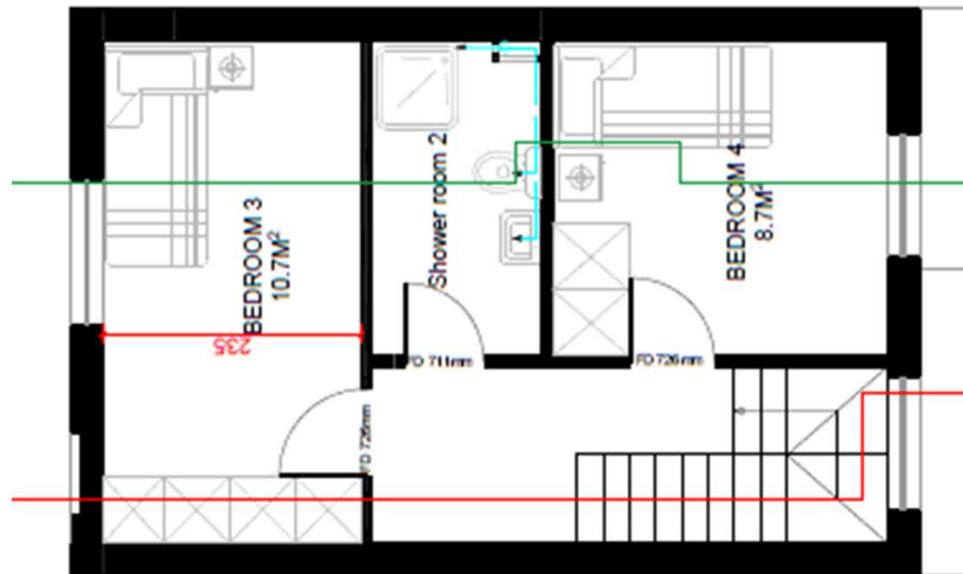


## Proposed first floor plan



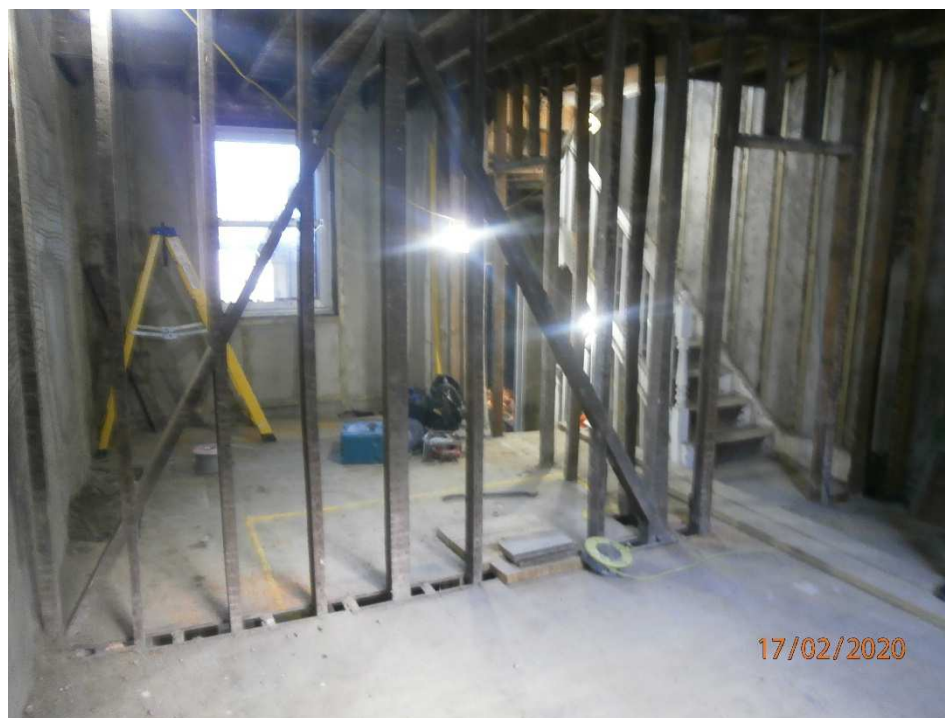


# Proposed second floor plan



# First floor bedroom 1

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# First floor bedroom 2

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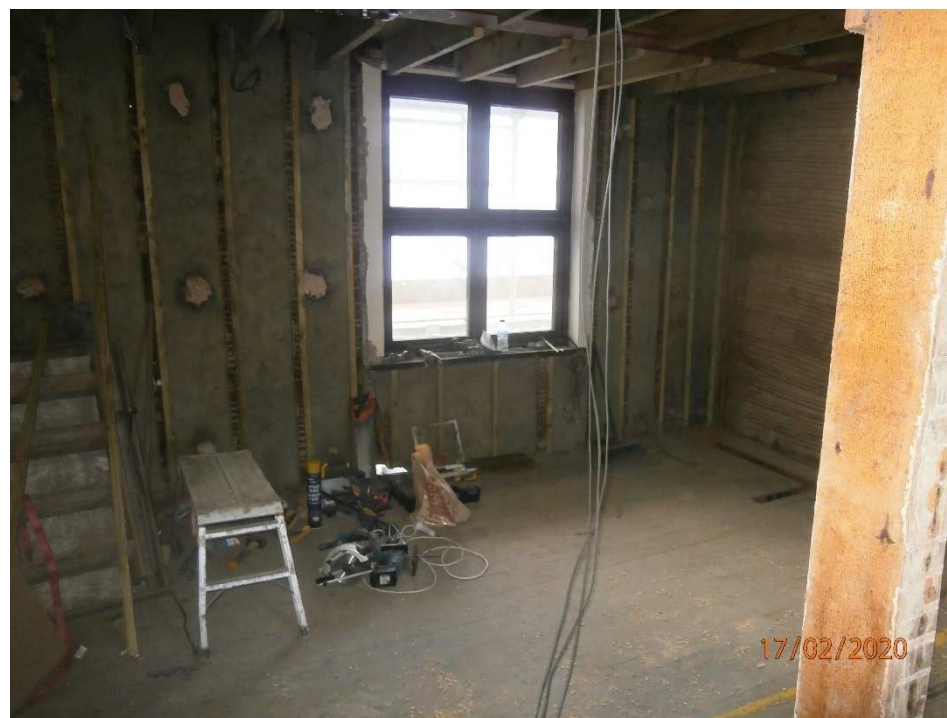
# Second floor bedroom 4

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# Second floor bedroom 3

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# CP21 Map

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# **Key Considerations in the Application**

- Principle of the Change of Use
- Standard of accommodation
- Neighbour Amenity



# Conclusion and Planning Balance

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- **Principle of the Change of Use**
  - 5 of 69 neighbouring properties within 50m are HMOs (7.25%)
  - This is in accordance with CP21 limit of 10%
- **Standard of accommodation**
  - Bedroom sizes are sufficient for a bed and storage furniture. All bedrooms would have circulation space, natural light and acceptable outlook.
  - The communal space is considered of sufficient size for 4 occupants.
- **Neighbour Amenity**
  - Similar level of occupation as existing use, so no significant harm to neighbour amenity
- **The proposed development is considered acceptable and in accordance with Development Plan Policies.**

**Avon Court,  
12 Dallington Road  
BH2020/00325**



**Brighton & Hove  
City Council**

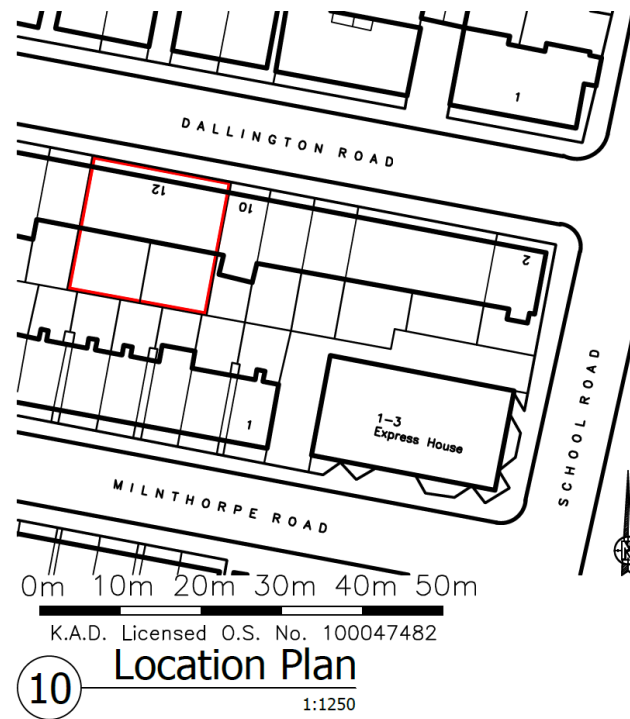
# **Application Description**

Planning permission is sought for the raising of ridge height involving a new roof construction, together with two front dormers and three rear roof extensions to form an additional two bedroom dwelling (Use Class C3).

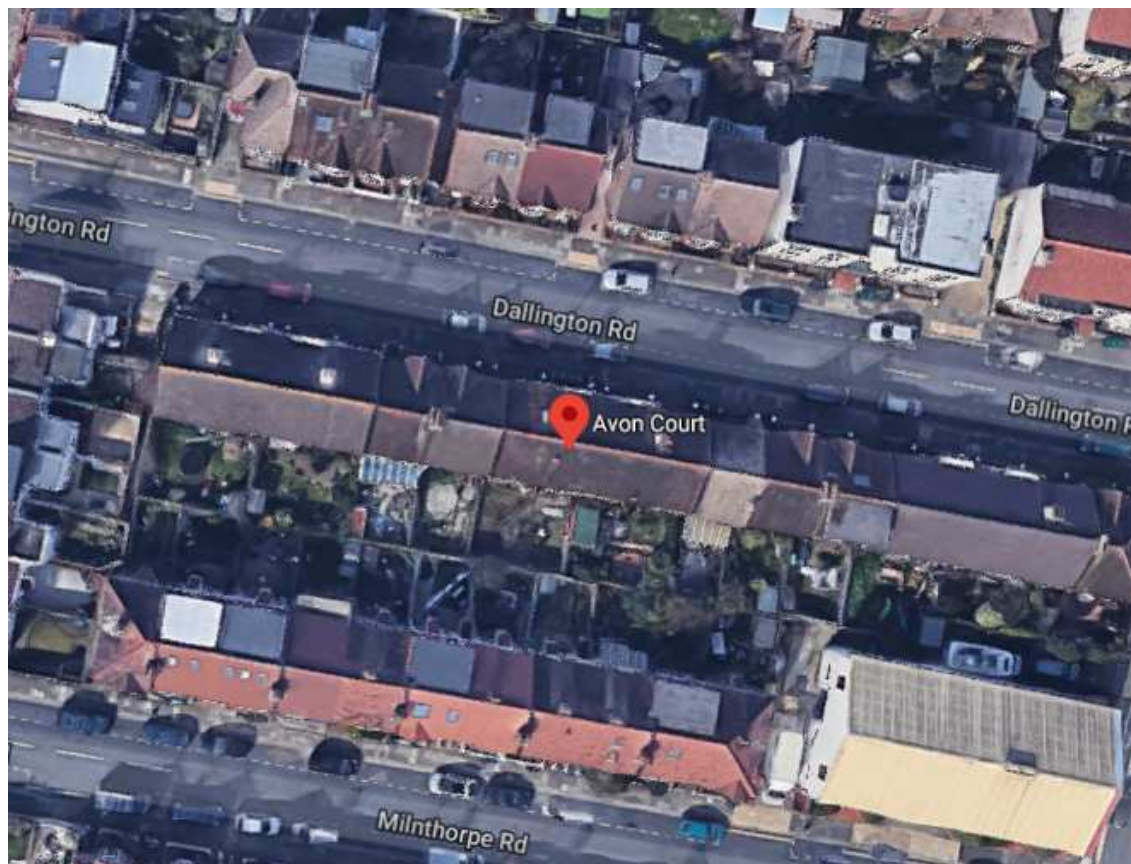
# Map of application site



# Existing Location Plan



# Aerial photo of site

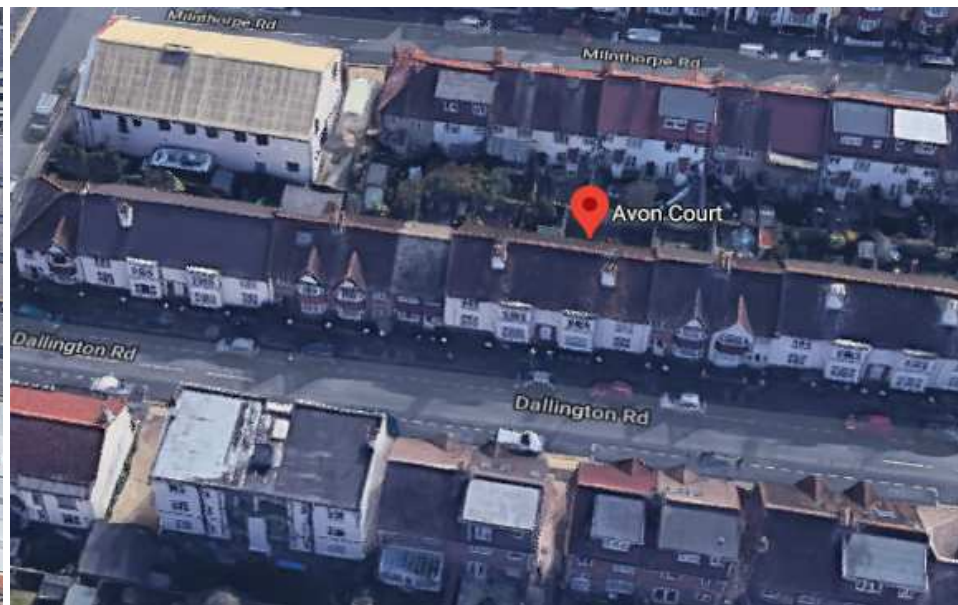




# 3D Aerial photos of site



**Rear**



**Front**



# Street photos of site

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# Street photos of site



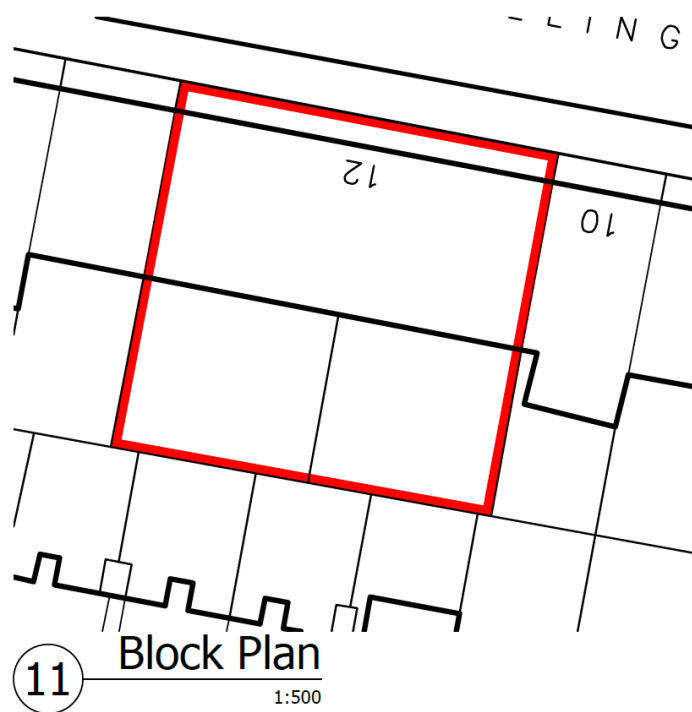
# Photos of rear of site from street



# Photos of rear of site from street



# Existing Block Plan





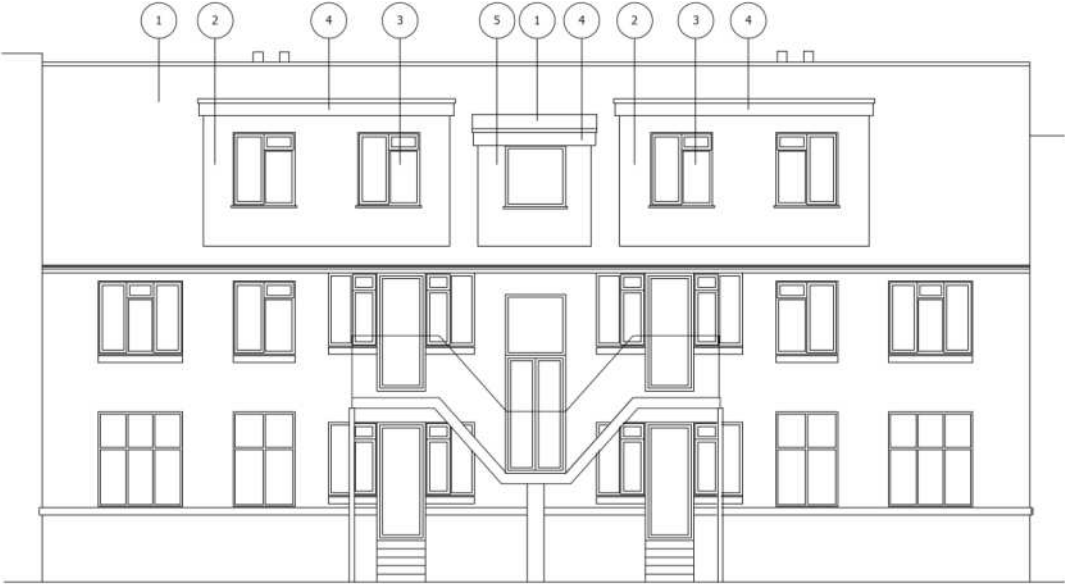
# Proposed Front Elevation



- 1. Clay tiles
  - 2. Tile hanging
  - 3. White uPVC windows
  - 4. White uPVC fascias
  - 5. Render
- F Front dormer 30/03/20 TKP revised
- E Ridge 20/03/20 TKP revised
- D Areas & dormers 19/03/20 TKP revised
- C Areas & dormers 12/03/20 TKP revised
- B Reduction to 30/01/20 TKP 1no. flat and revisions to roof
- A Bins/bikes 24/10/19 TKP

9 Proposed Front Elevation  
Scale: 1:100

# Proposed Rear Elevation



- 1. Clay tiles
  - 2. Tile hanging
  - 3. White uPVC windows
  - 4. White uPVC fascias
  - 5. Render
- F Front dormer 30/03/20 TKP revised
- E Ridge 20/03/20 TKP revised
- D Areas & dormers 19/03/20 TKP revised
- C Areas & dormers 12/03/20 TKP revised
- B Reduction to 30/01/20 TKP 1no. flat and revisions to roof
- A Bins/bikes 24/10/19 TKP

8 Proposed Rear Elevation  
Scale: 1:100



# Existing and Proposed Streetscenes



Brighton & Hove  
City Council

# Key Considerations in the Application

- The principle of development
- The scale, appropriateness and external appearance of the extensions and raised ridge height
- Overlooking of neighbouring properties from the windows within the extensions
- The standard of accommodation offered by the flat
- Car and cycle parking

# **Conclusion and Planning Balance**

- Following revisions, the extensions are not considered to be incongruous within the surrounds area and are designed to fit in with and be adequately subordinate to Avon Court.
- No significant additional harm would be caused to neighbouring properties through overlooking or overshadowing.
- The new flat offers acceptable living conditions
- Parking permits would be restricted by condition and a cycle parking space is provided.
- As such, the development is considered to be acceptable and is recommended for approval.

# **Black Rock Site and Surroundings, Madeira Drive**

**BH2020/00442**



**Brighton & Hove  
City Council**

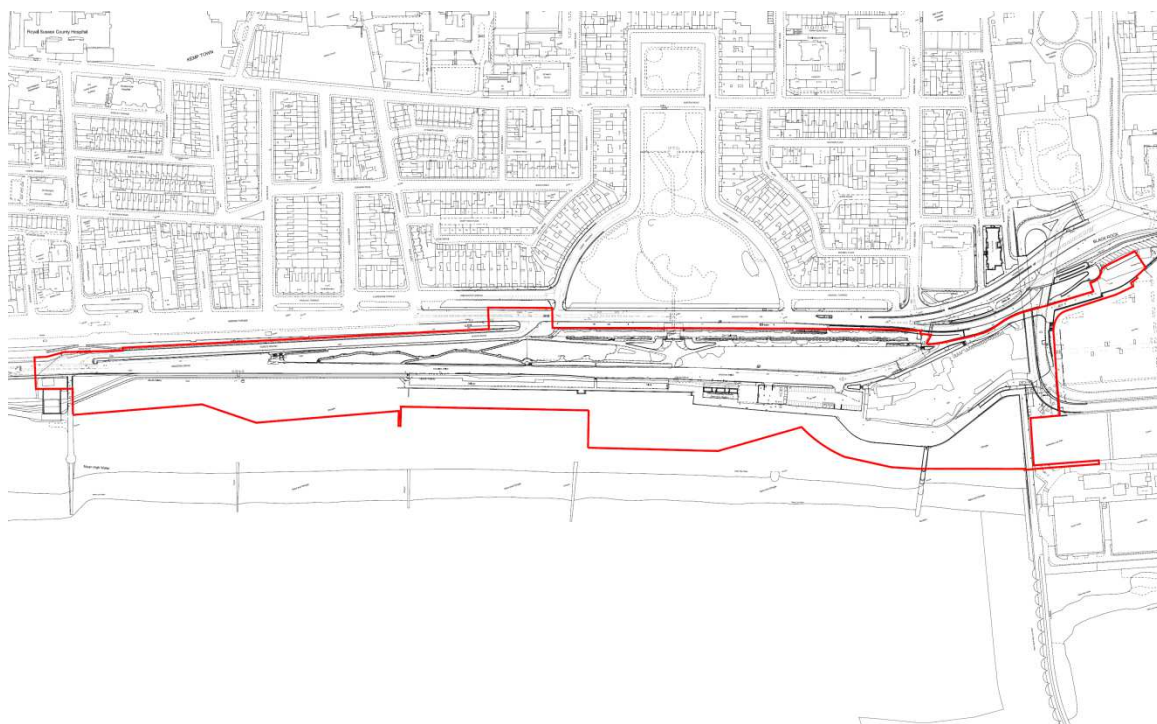
# Application Description

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Replacement of existing sea wall with a realigned free-standing structure; the formation of an access route from Black Rock extending to Brighton Marina; enhancement of highways infrastructure for Duke's Mound at its junctions with Marine Parade and Madeira Drive; restoration of The Old Reading Room and The Temple and change of use for flexible A1, A3, D1 or D2 Use; widespread enhancement of public realm for pedestrians and cyclists via new amenities, facilities and landscaping, with associated ecological enhancement.

# Existing Location Plan

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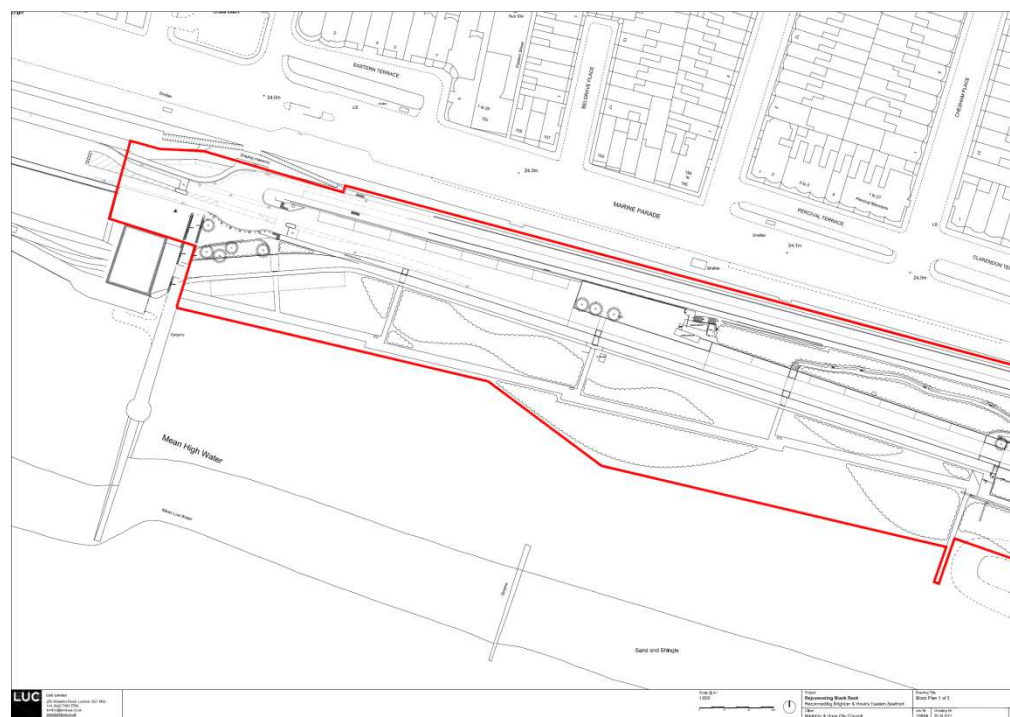


Brighton & Hove  
City Council

ID

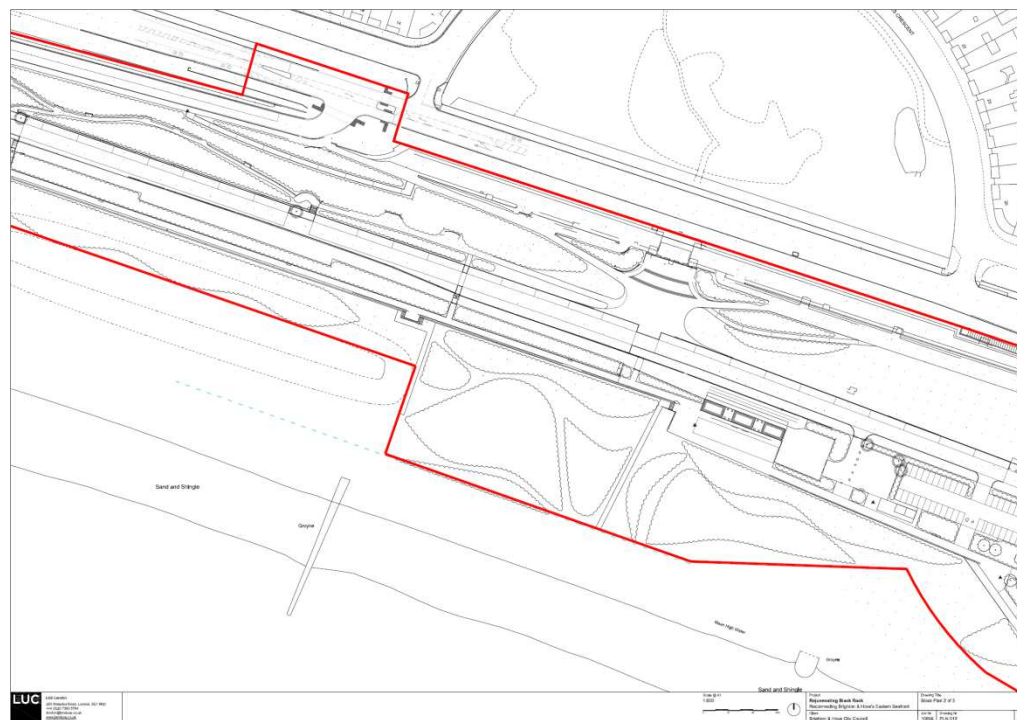


# Existing Block Plan



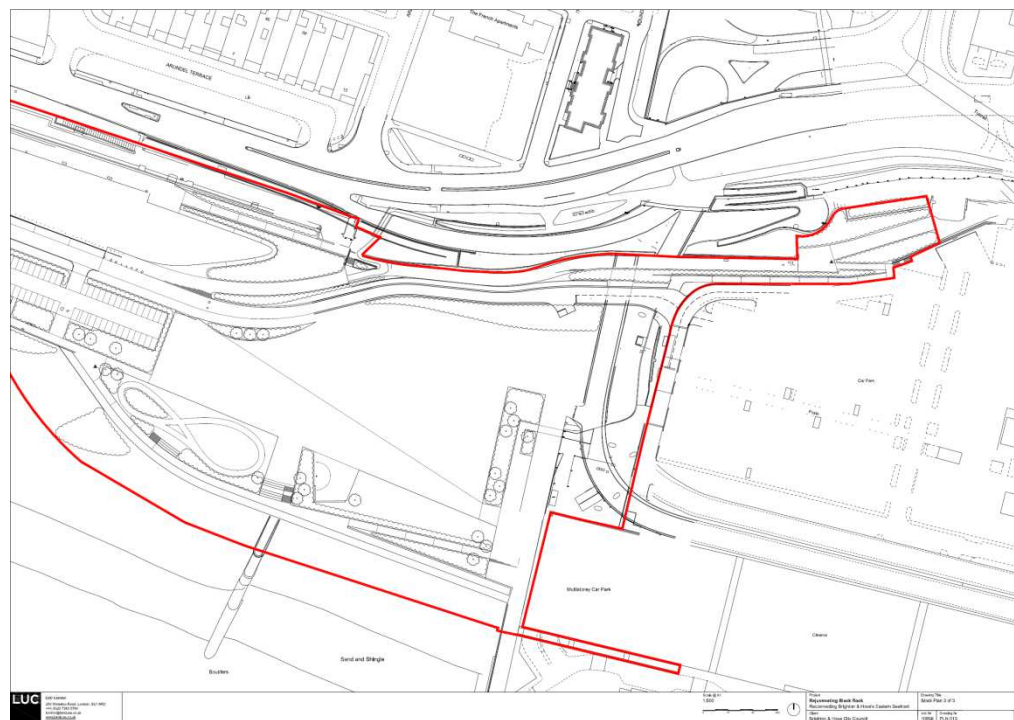
10856-PLN-011

# Existing Block Plan



10856-PLN-012

# Existing Block Plan



10856-PLN-013

# Aerial photo(s) of site



Brighton & Hove  
City Council



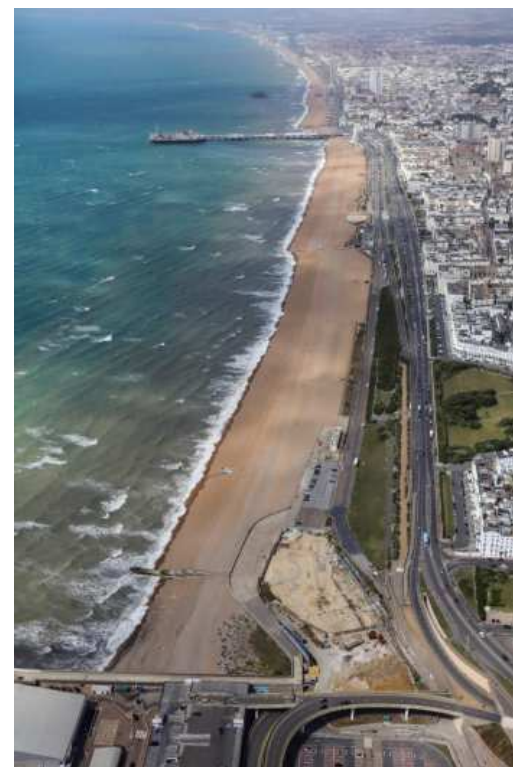
# 3D Aerial photo of site

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Brighton & Hove  
City Council

# Aerial Image of application site



# **Black Rock as Existing**





# Black Rock Site - Existing

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Existing space on development site



View From North

Black Rock car Park

# Existing Access

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Southern Black Rock ramped access to Marina

Northern Black Rock access to Marina



# Heritage Assets

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Esplanade Cottages



The Old Reading Room



The Temple

## Setting of Site towards North– Lewes Crescent

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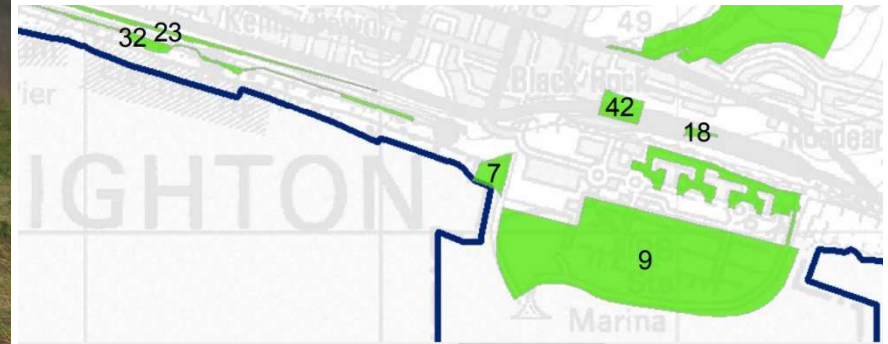
# Black Rock LWS

---



## LWS within/in Proximity to Site

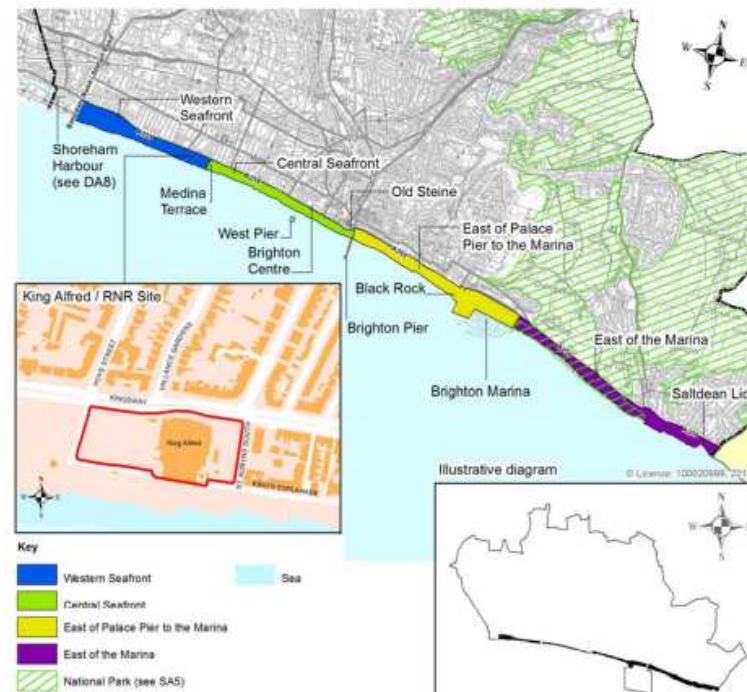
- 7 - Black Rock Beach
- 9 - Brighton Marina
- 18 - Cliff Corner
- 32 - Volks Railway
- 33 - Surrenden Field Copse
- 42 - Cliff Road Paddock



Brighton & Hove  
City Council

# Policy Map – SA1

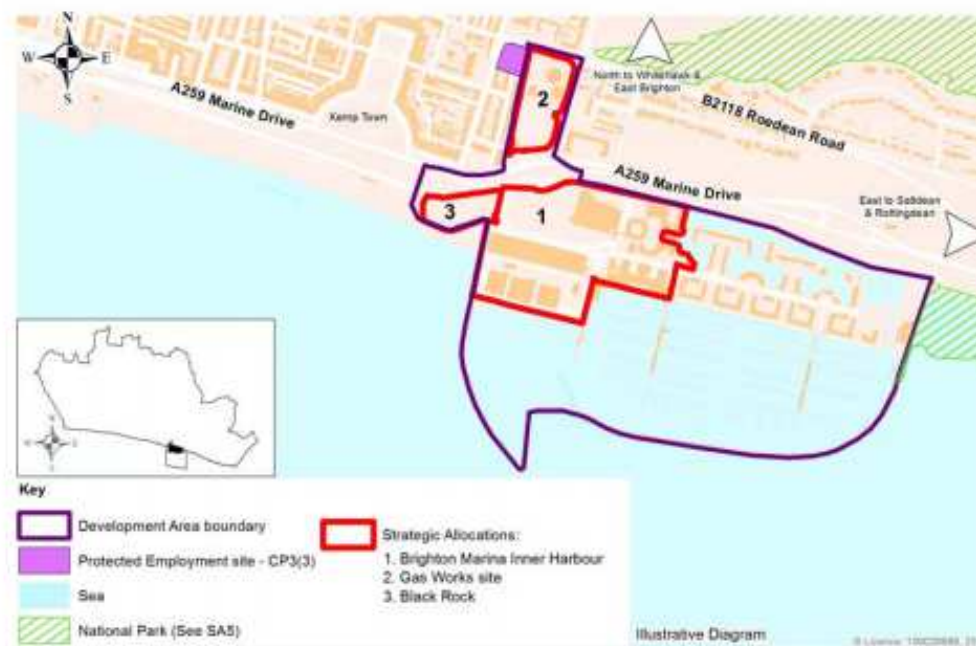
SA1 - The Seafront



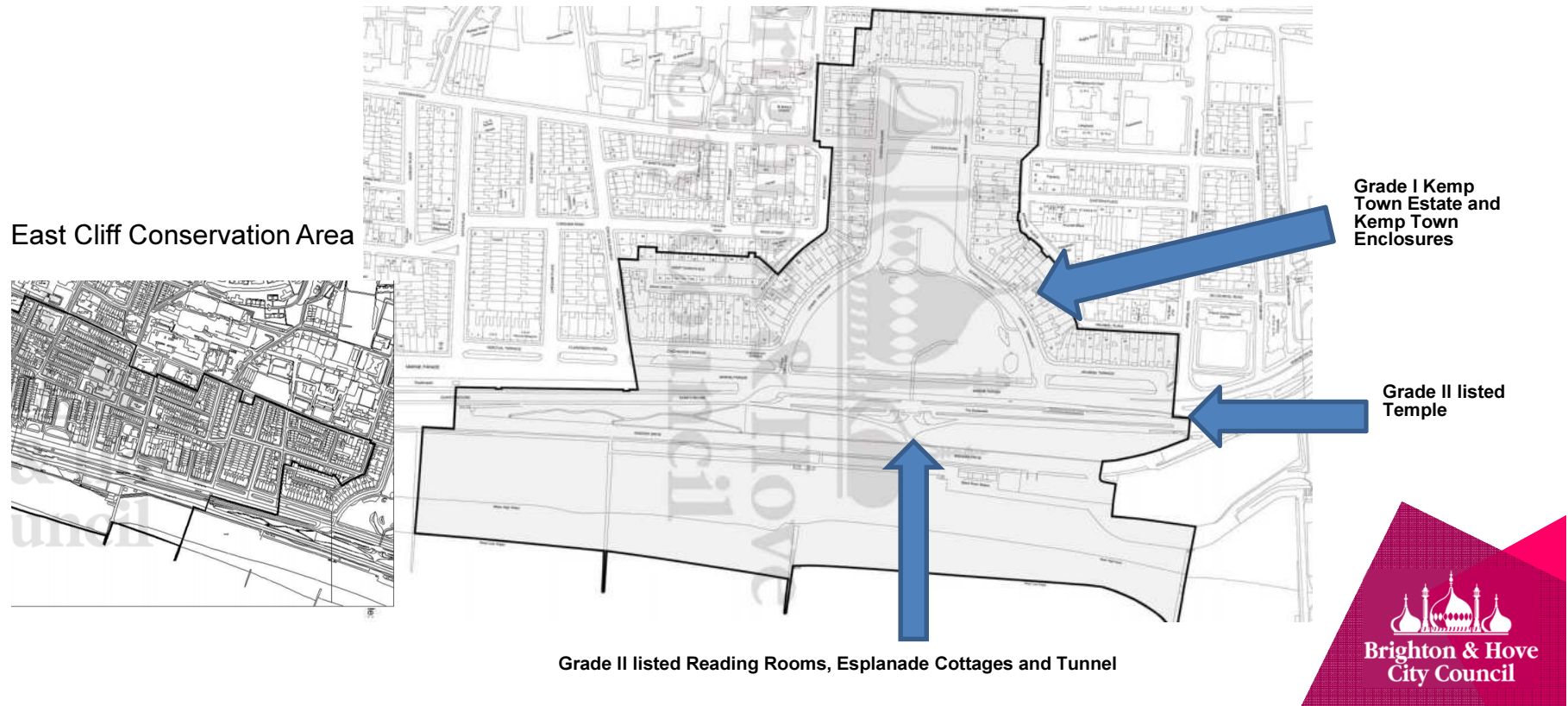


# Policy Map – DA2

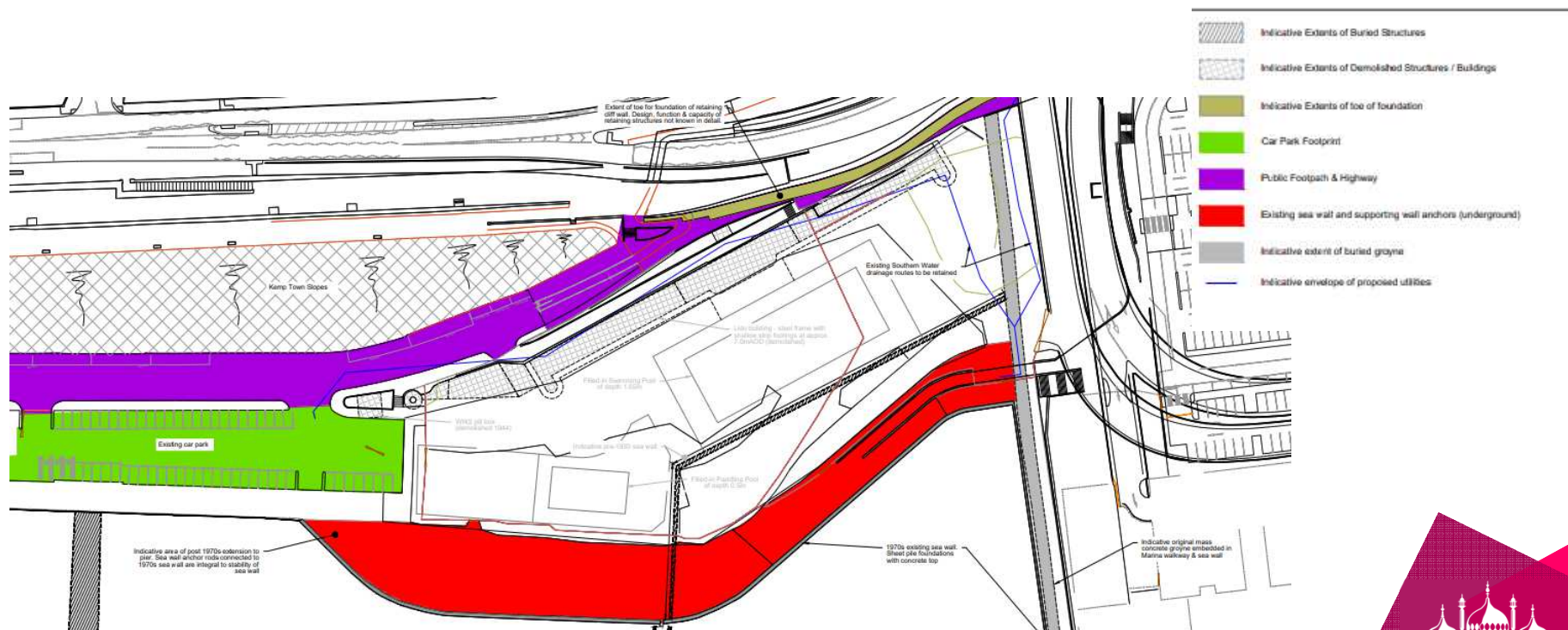
DA2 Brighton Marina, Gas Works and Black Rock Area



# Kemp Town Conservation Area

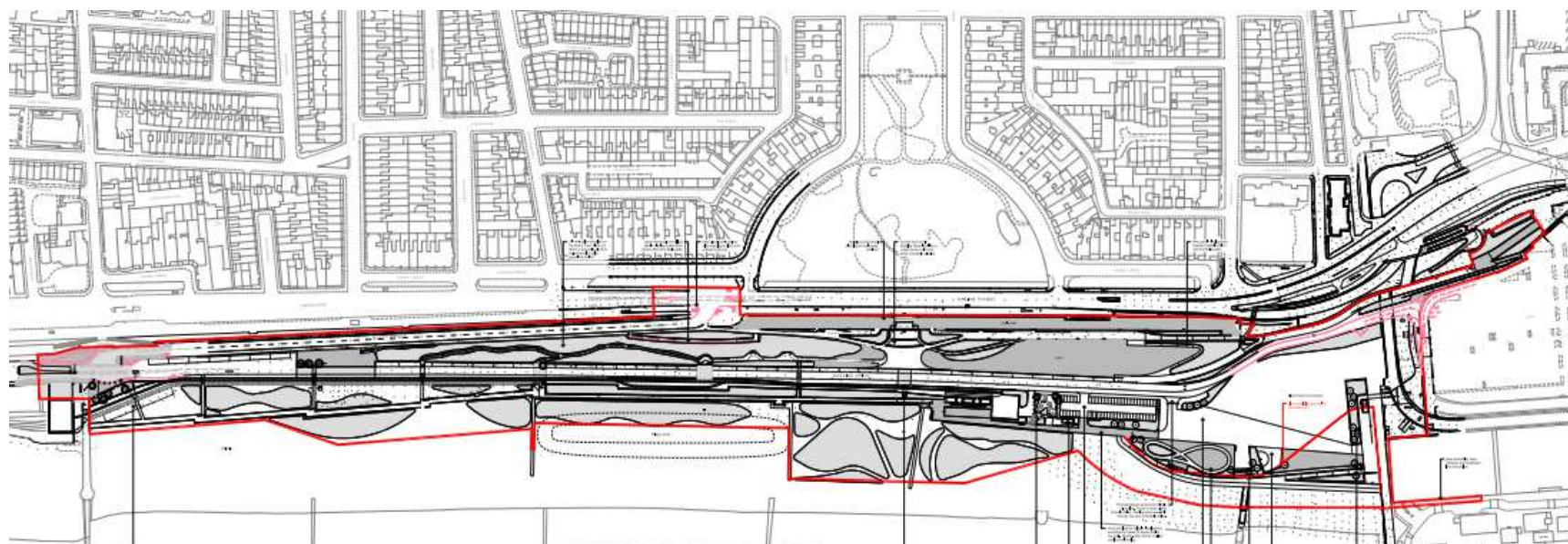


# Existing Site Access





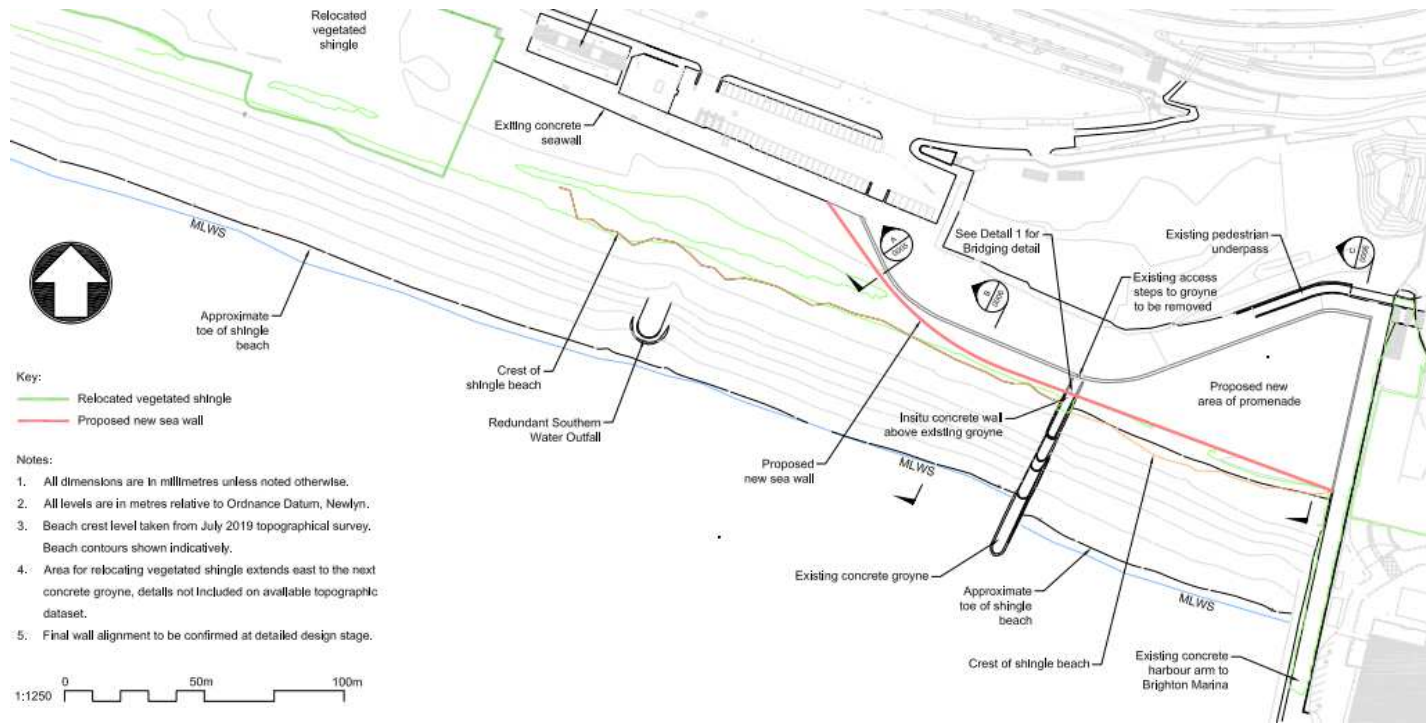
# Proposed Location Plan



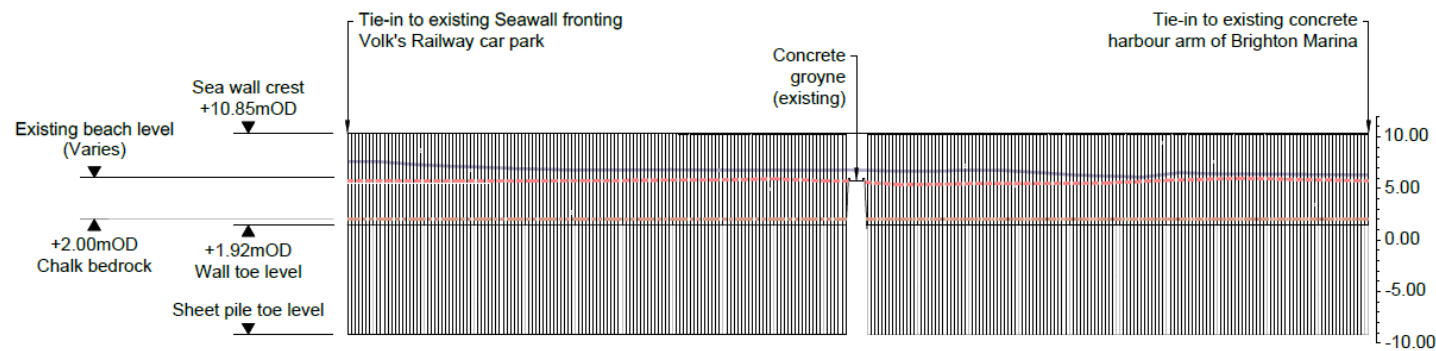
Brighton & Hove  
City Council

ID

# Proposed General Arrangement Plan

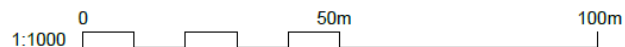


# Proposed Sea Wall Elevation



Elevation on front of proposed new Sea Wall

1:1000H 1:2000V

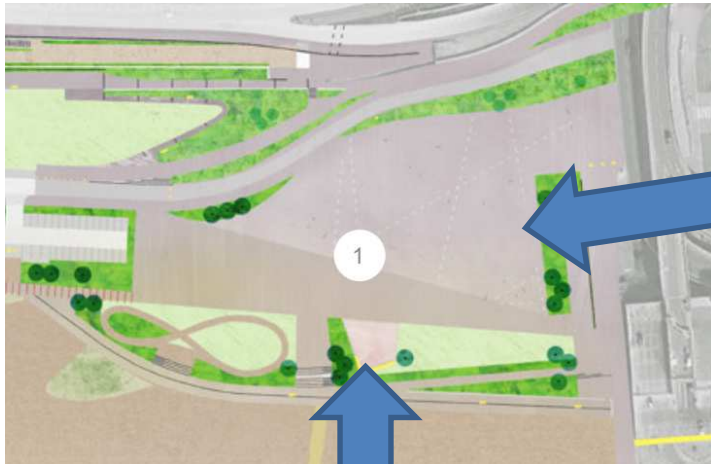


Key:

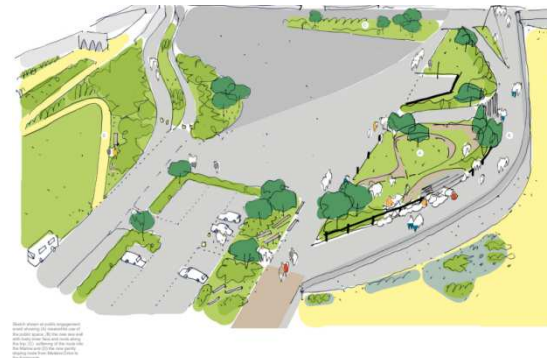
- Existing beach level shall be retained upon completion of works
- Existing promenade level
- Chalk bedrock (Approx level)



# Black Rock Proposals



- Realignment of Sea Wall
- Clearance, remediation and preparation of site for future d
- New access link between Marina and Madeira Drive
- Improvements to vehicle, cycle and pedestrian links



The following temporary uses are also proposed within Black Rock:

Creation of temporary events space;  
Provision of temporary pump track;  
Provision of temporary recreation space with half court and basketball net

# Western End of Site

Madeira Drive improvements for pedestrians and cyclists

Old Reading Room Commercial Use, improved pedestrian access, landscaping

New gently sloping route



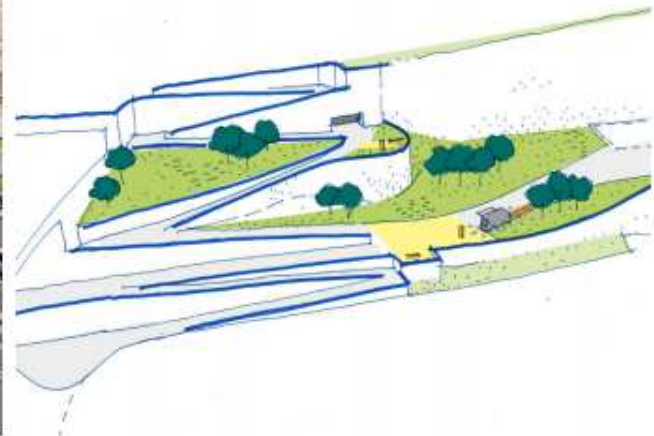
New Boardwalk, access routes, vegetated beach habitat

# Undercliffe Walk

Marina access  
link for  
pedestrians  
(future CTR  
link)



Longer ramped access up to  
existing undercliff walk, and  
steps down to car park



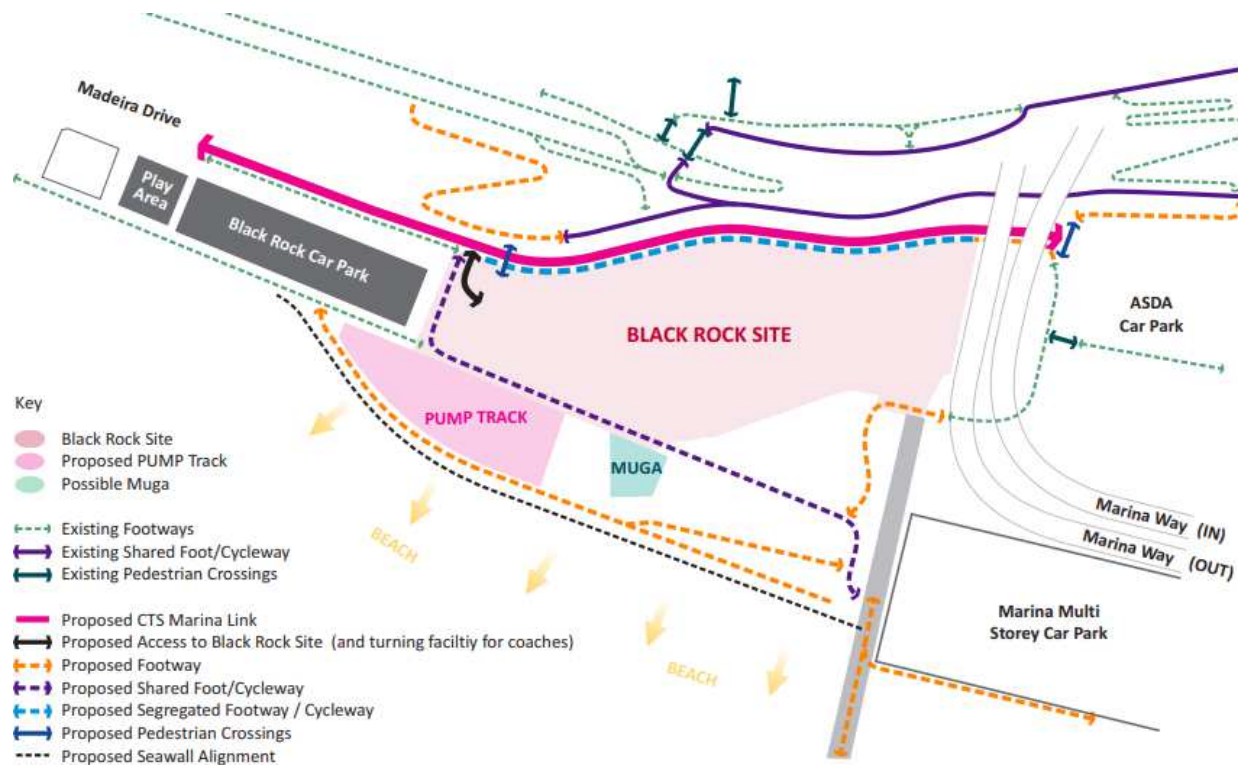
New Planting, re-aligned ramp

# Proposed Access

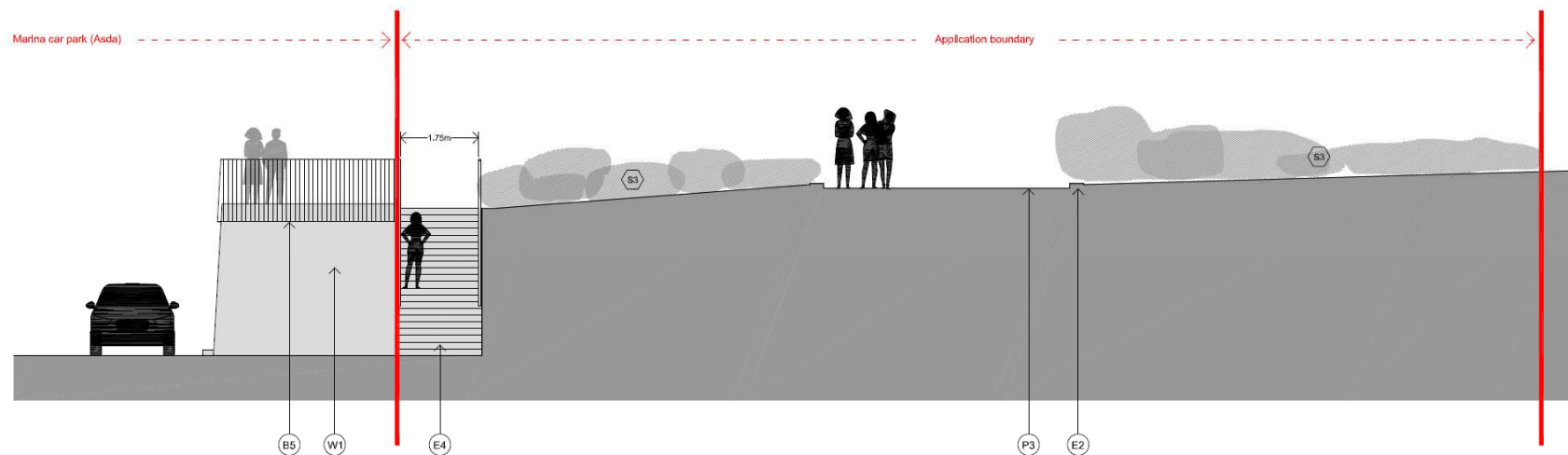




# Black Rock Access Plan



# Section to Marina Car Park steps



1 Section to marina car park steps  
Scale 1:100 @ A2

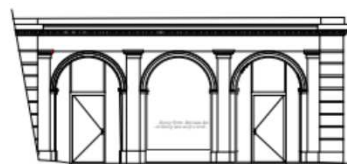
10856-SEC-603



# The Temple

The Temple, Kemp Town, Brighton  
Proposals: Creation of Weatherproof Shell

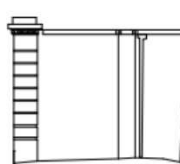
## PROPOSED



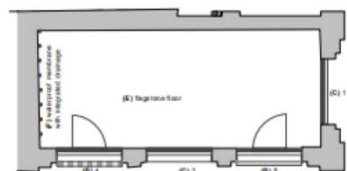
Proposed: WEST ELEVATION



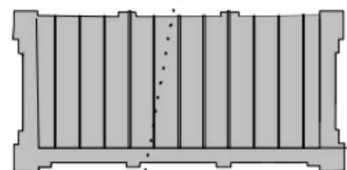
SOUTH ELEVATION



EAST ELEVATION



FLOOR PLAN



ROOF PLAN

### PROVISION OF NEW GLAZED OPENINGS

- (A) Carefully remove concrete ramps to openings and form new threshold drain as DWG022
- (B) Doors: install structural glass to form door openings with level threshold to openings 2 & 4. in IQ minimal glass or similar alternative, with powder coated SS frame RAL: 7015. 1000mm clear opening. Set back from face of building.
- (C) Windows: install 2 no. frameless structural glass windows to openings 1 & 3. IQ minimal glass or similar alternative, with powder coated SS frame RAL: 7015 and integrated kick frame. Set back from face of building.

**Essential Structural Repairs and Maintenance:** undertake essential repairs and consolidation works in accordance with Historic England guidance, and prepare shell for re-use including:

- (D) Following detailed inspect of render and stonework, undertake repairs using hydraulic lime to match original, carefully remove all flaking and loose modern paintwork and decorate in Keim mineral paint to match Kemp Town Terraces over in BS 0055
- (E) Carefully lift modern concrete block flooring material, subject to substructure, carefully excavate and form new substructure. Lay new natural flagstones on new sub-base
- (F) Provide waterproof membrane with integrated drainage system (Newton or Delta membrane) to internal walls, which abut concrete structures on north and part of east elevation together with integrated drainage.
- (G) Subject to detailed roof survey, position and fix new CI neg to allow outflow into adjacent drainage system. Form base for and lay new Code 7 lead roof, with parapet cover flashing, in accordance with LSA detailing including anti-theft system. Drainage system to form integrated part of enhancement proposals to Esplanade and pathways designed by others.
- (H) All for provision of utilities - power, data, water.
- (I) Integration of any recommendations made in Structural Engineer's report
- (J) Following fitting of new floor and glazing allow for period of drying out and monitoring of structure before undertaking redecoration and/or fit out works which may be subject to separate Listed Building Application.

**Principal Consultants**  
NTR Planning

**Listed Building Proposals**  
Marlow Conservation Limited

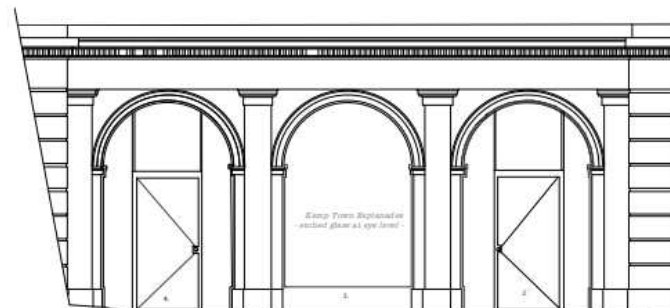
Tel: 01646 636270  
Mob: 07936 63424  
Email: clared@ntrplanning.co.uk

**Project:** The Temple  
Black Rock Enhancement Proposals

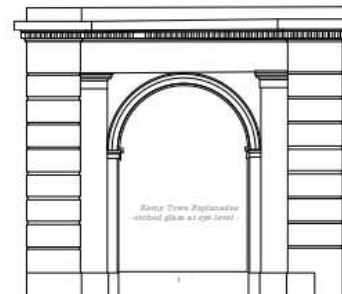
**Client:** Brighton & Hove City Council  
**Scale:** 1:100  
**Dwg:** 01 Proposed Plans and Elevation  
The Temple  
LISTED BUILDING APPLICATION

Date of issue: 17th January 2020

Site survey: M J Zare Associates



Proposed: WEST ELEVATION



Proposed SOUTH ELEVATION

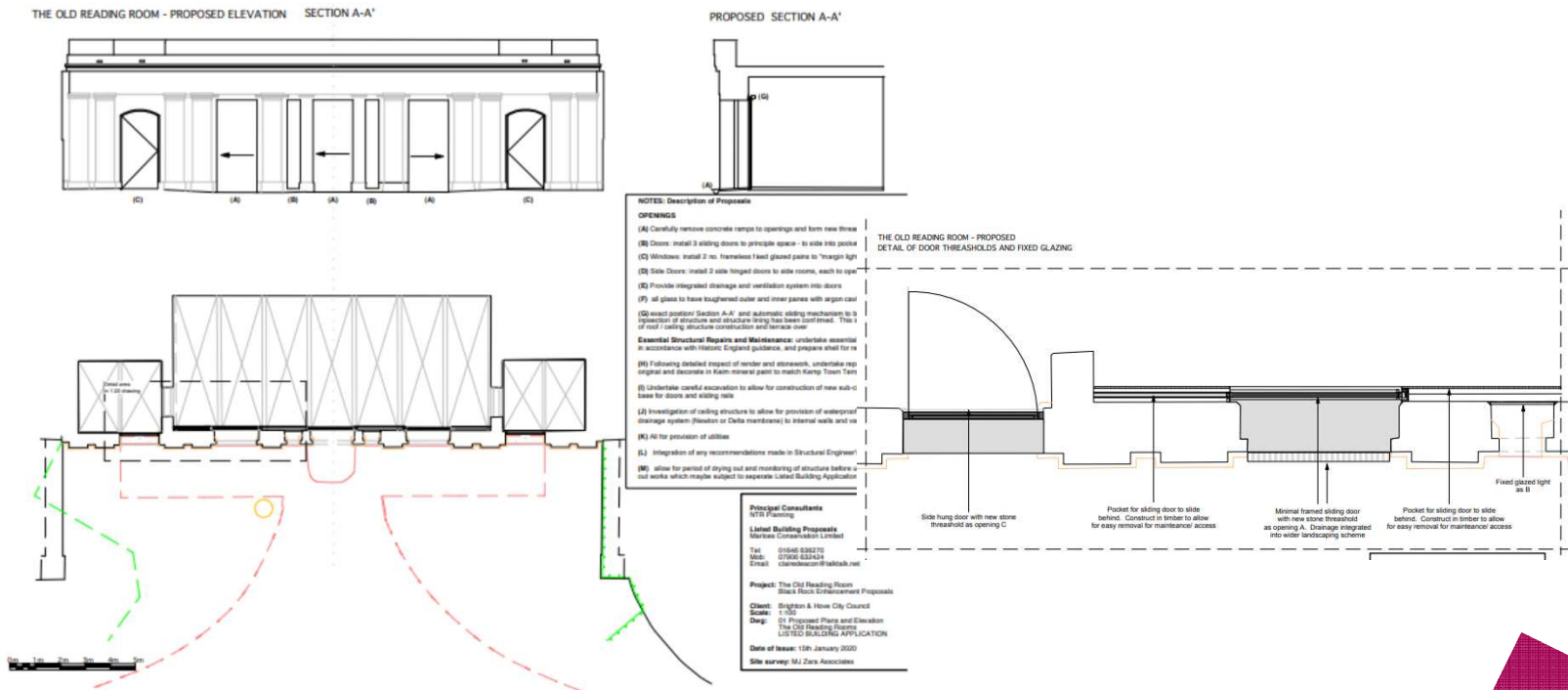
### NEW GLAZED OPENINGS: DETAIL

- (A) Carefully remove concrete ramps to openings and form new threshold drain to run into drainage system designed by others
- (B) Doors: install structural glass to form door openings with level threshold to openings 1 & 2. in IQ minimal glass or similar alternative
- (C) Windows: install 2 no. frameless structural glass windows to openings 1 & 3. IQ minimal glass or similar alternative. 5 of sets to match original stone base
- (D) 30mm Frames carefully formed around render detailing
- (E) Door Frames 50mm at hinge, 50mm at closing point and 132 mm around door lock
- (F) Cranked handle with automatic opener integrated
- (G) Door and window frames powder coated stainless steel in dark grey RAL: 7015



Brighton & Hove  
City Council

# Old Reading Room



Brighton & Hove  
City Council



# Key Considerations in the Application

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- Principle of development - Site allocation within Policy DA2
- Impact on the visual amenities of the public areas, the Heritage Assets, the street scene and the wider Conservation Areas, as well as the setting of listed buildings within the locality of the site.
- Impact on highways,
- Ecology
- Trees
- Neighbouring amenity
- Sustainable drainage.

# S106 table

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- Highways Works (requested by Local Highway Authority):
  - All works within the existing highway to be subject to approval as LHA.
  - The highway works on private land are to be adopted as Highway Maintainable at Public expense.
  - The Highway works for the CTR Link shall include an area of Highway Verge to facilitate a suitable design of cycleways for the permanent design solution.
- The development should not be occupied until the scheme of highway works is implemented (on a phased basis where appropriate).
- Public Art: Commissioning and installation of an Artistic Component. Proposed in the form of 'Telling the story of Black Rock and the seafront' with a coherent programme of wayfinding, interpretation and public art.
- Submission of an Employment & Training Strategy to secure the use of at least 20% local construction labour,

# Conclusion and Planning Balance

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- The proposal is welcomed as it will enable future regeneration of the derelict Black Rock site and implementation of a strategic allocation in the adopted plan. It will also provide significant improvements in links to the Marina.
- Details of the proposal and supporting evidence has demonstrated that the proposed realignment of the sea wall is required in order to deliver the strategic allocation and allow future regeneration of the Black Rock site. The loss of the existing Black Rock SNCI/LWS is regrettable, however, it is considered to be satisfactorily offset by the proposal to relocate the vegetated shingle and Management Plan.
- The County Ecologist has noted the details within the draft Management Plan, and the proposed net gain in Biodiversity within the proposal. A full Ecological Design Strategy is required by condition.
- Improvements to the public realm in this section of the seafront are welcomed. The proposal represents an opportunity to enhance the present situation in respect of the designated heritage assets and the surrounding public realm, as well as improving on the connectivity around the site. The Highway Authority has no objection subject to recommended conditions and obligations.
- The future maintenance of the proposal is necessary to ensure the acceptability of the scheme., Details of management and future maintenance of the scheme are required by condition.



# 20-22 Gloucester Place

**BH2020/00699**

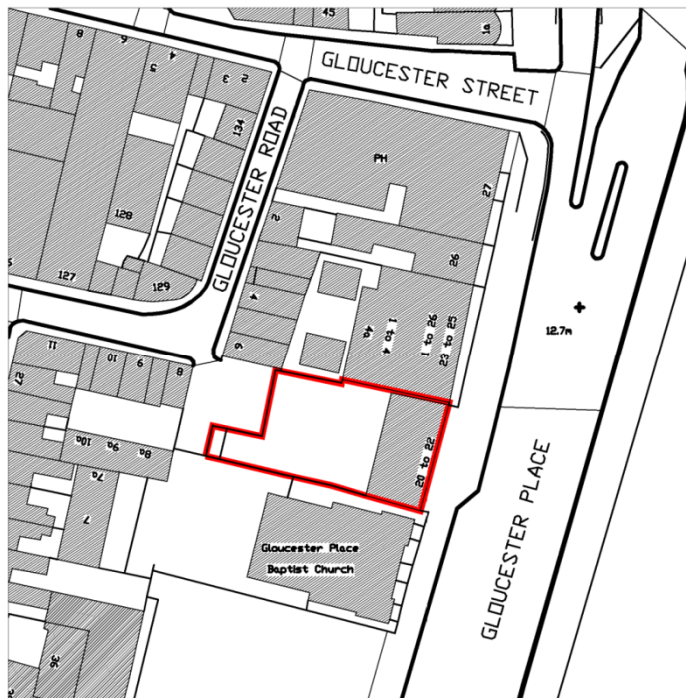


**Brighton & Hove  
City Council**

# Application Description

- Erection of two storey roof extension to form 4 no. additional dwelling units;
- 1x One-bed dwelling, 1x Two-bed dwelling and 2x Open plan studio;
- 12 cycle parking spaces, 7 car parking spaces including 2 disabled.

# Existing Location Plan



**Existing site location plan**  
Scale 1:1250@A3



Brighton & Hove  
City Council

ID

# Aerial photo of site



No. 26 - Grade II  
listed building

Locally listed  
Baptist Church



Brighton & Hove  
City Council

# 3D Aerial photo of site

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# Site Photos – Existing Front and Rear



Brighton & Hove  
City Council



# Site Photos – Existing Streetview



Brighton & Hove  
City Council

## Site Photos - Views from Gloucester Road to rear



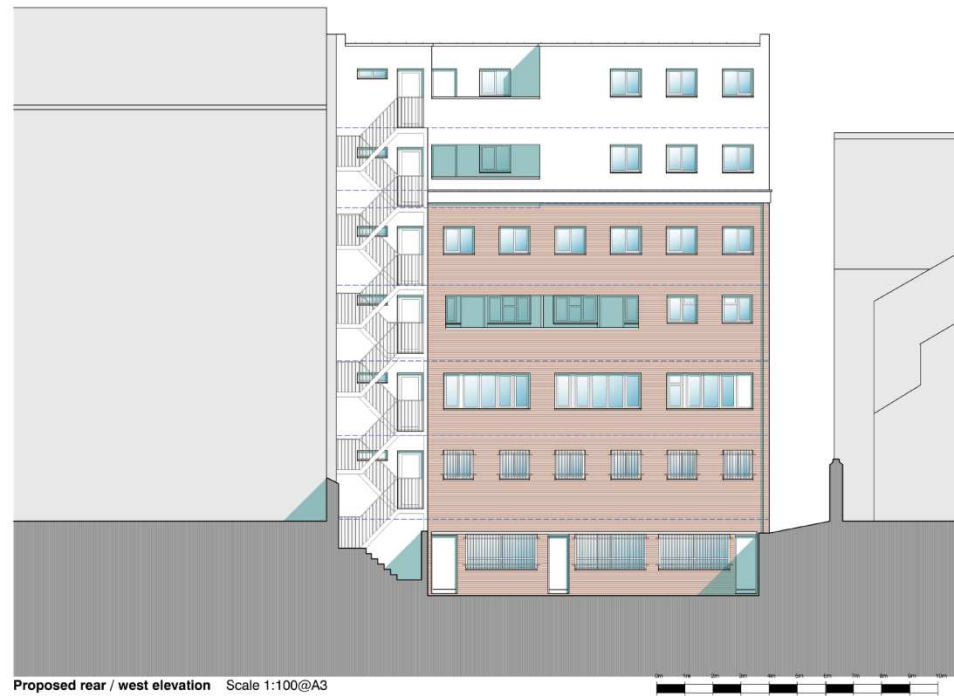
Brighton & Hove  
City Council

# Proposed Front Elevation



TA 1255/27 A

# Proposed Rear Elevation



# Proposed Streetview

Approved development at The Astoria reference BH2019/ 00199



Comparative street scene - proposed east elevation Scale 1:200@A3

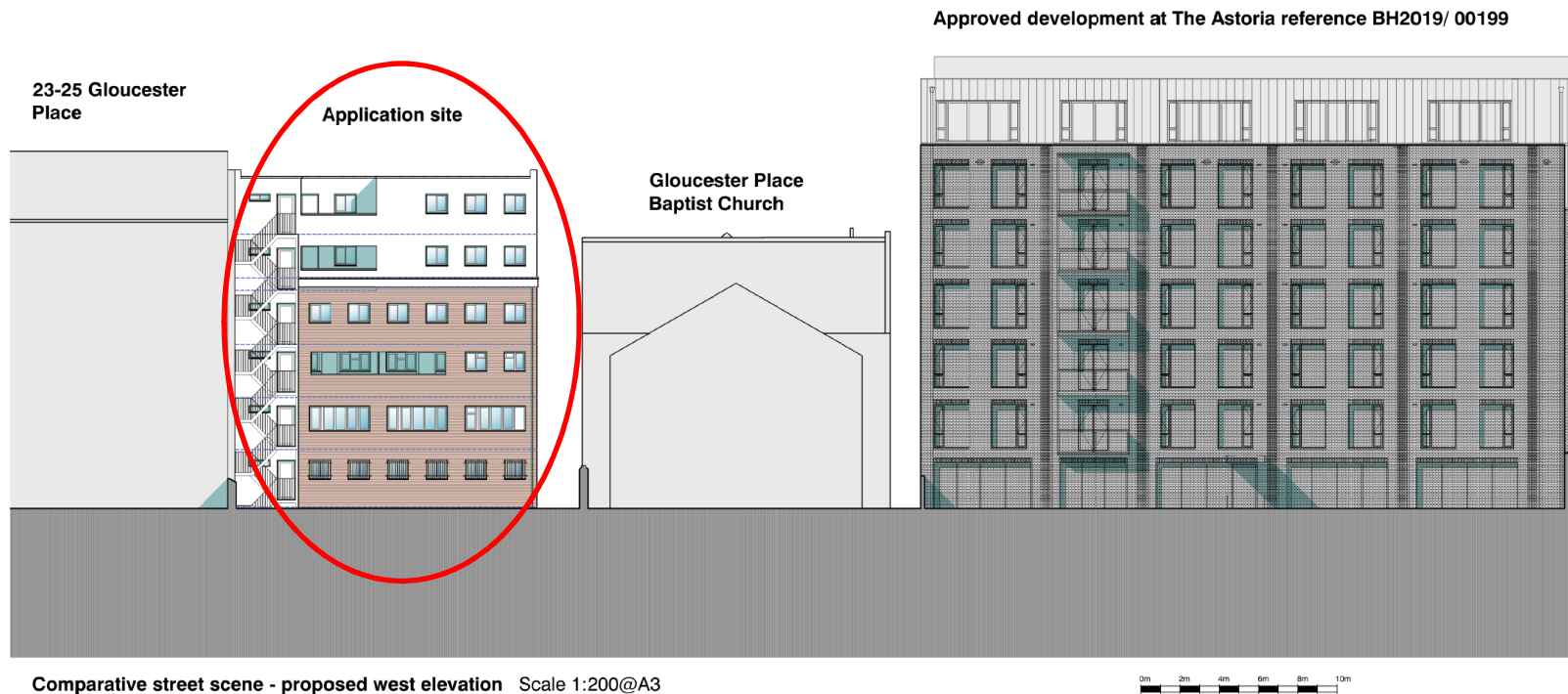


Brighton & Hove  
City Council

ID



# Proposed Streetview (rear)



ID



# Key Considerations

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- Principle of the development;
- Impact of the character and appearance of the building, conservation area and setting of nearby listed buildings;
- Standard of accommodation;
- Neighbour amenity;
- Highways issues.

# Conclusion and Planning Balance

- Provision of 4 new dwellings for the City;
- No harm to the character and appearance of the building, conservation area or setting of nearby listed buildings;
- All units would comply with Govt. space standards and benefit from good levels of light, outlook and circulation space;
- No significant impacts on adjoining occupiers in terms of loss of privacy, light and outlook
- Proposed vehicle (including disabled) and cycle parking provision is acceptable.

# 2 Dyke Close

**BH2020/00724**

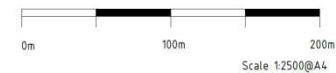
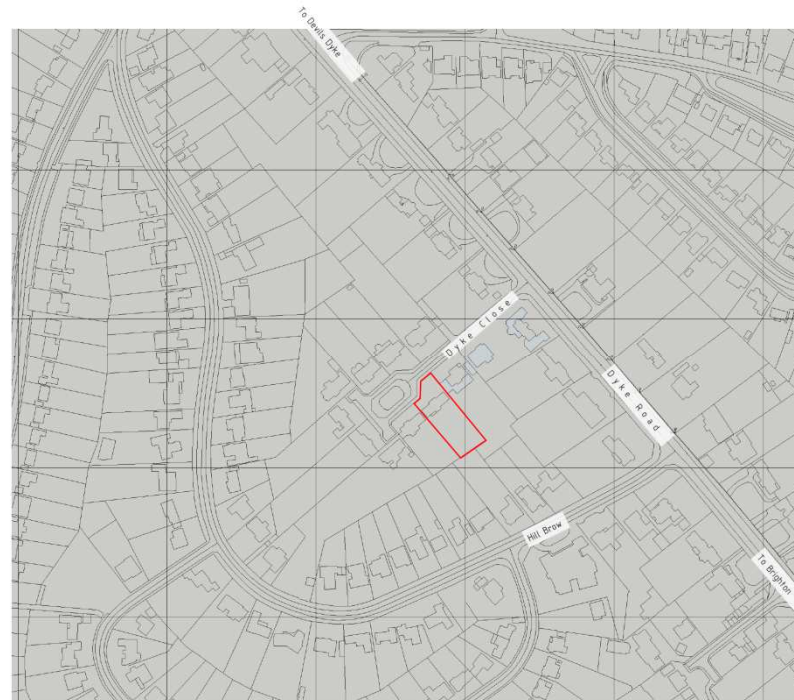


**Brighton & Hove  
City Council**

# Application Description

- Erection of two-storey rear extension incorporating 2no first floor balconies. Roof alterations including 2no front dormer windows, rear dormer featuring balcony, and removal of existing chimneys. Repositioning of garage, alterations to fenestration and associated works.

# Existing Location Plan



Location Plan



Brighton & Hove  
City Council

ID

# Aerial photo(s) of site

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# 3D Aerial photo of site

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# Street photo(s) of site





# Other photo(s) of site



Brighton & Hove  
City Council

# Other photo(s) of site

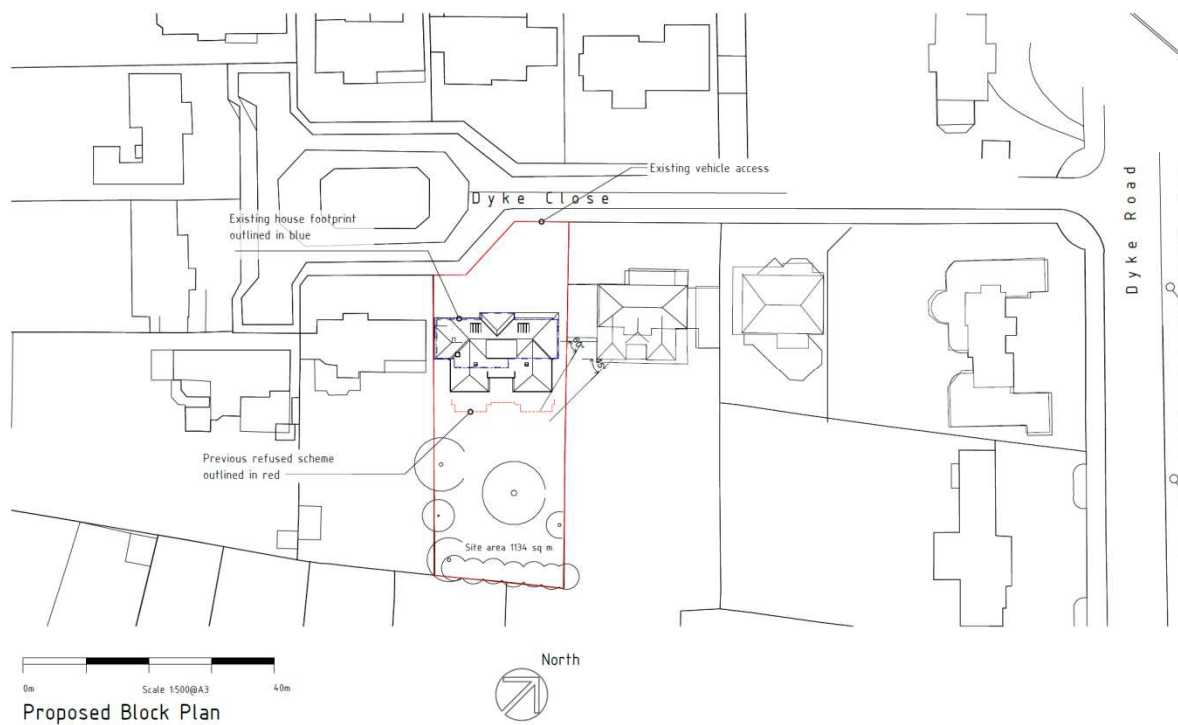


Brighton & Hove  
City Council

# Other photo(s) of site



# Proposed Block Plan



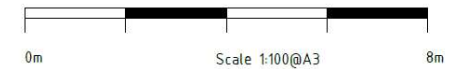


# Proposed Front Elevation



07 K

# Proposed Rear Elevation



Facing brick extensions to match existing for colour and texture and pointing

Period reconstituted stone surrounds and features

All proposed windows in white crittal period windows

Proposed South East Elevation

08 K

# Conxtextual Rear Elevation

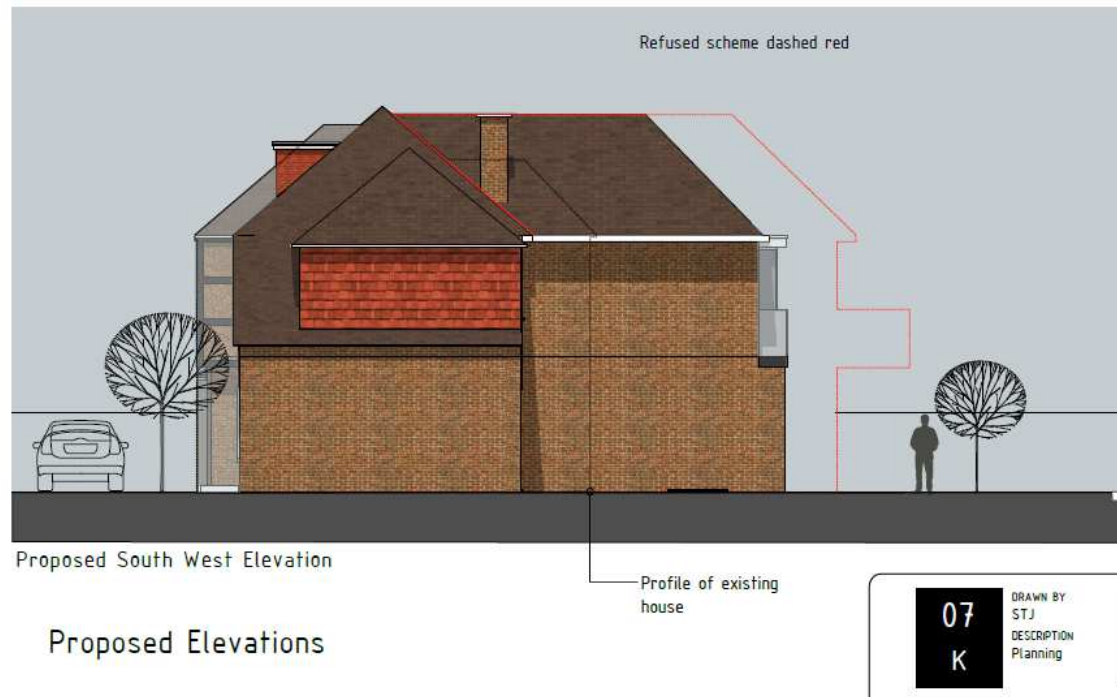
## Proposed Context Elevation



Rear  
South East Elevation

09 J

# Proposed South Side Elevation



Proposed Elevations

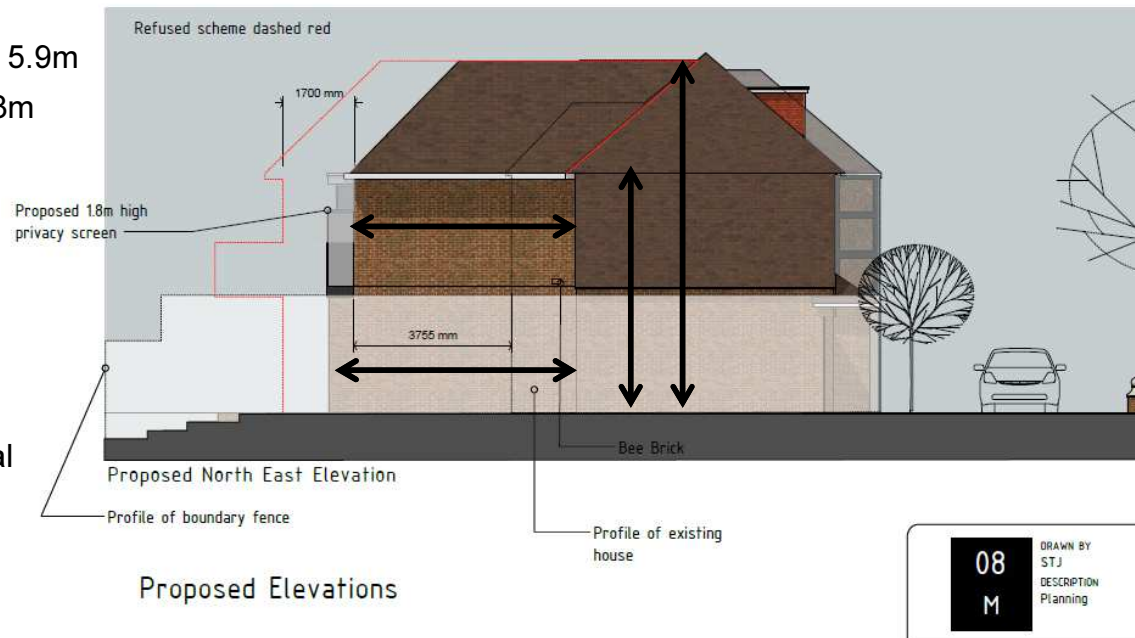
08 K

# Proposed North Side Elevation

Ground Floor Depth = 5.9m  
First Floor Depth = 5.3m  
Eaves Height = 5.8m  
Ridge Height = 8.4m

Reduction of approx.  
1.1m at ground floor  
from previous Proposal

Reduction of approx.  
1.7m at first floor from  
previous proposal



08 M

# Proposed Visual – Front Elevation



Brighton & Hove  
City Council



# Proposed Visual – Rear Elevation



Brighton & Hove  
City Council

# Key Considerations in the Application

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- Impact(s) on the amenities of residents of No.1 Dyke Close

# Conclusion and Planning Balance

- Previous, similar application refused on basis of harm to amenities – then dismissed at appeal.
- The current scheme is not a significant enough reduction to sufficiently mitigate this harm.



# 6 Princes Crescent

**BH2020/00776**



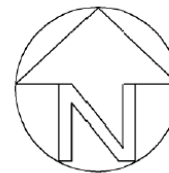
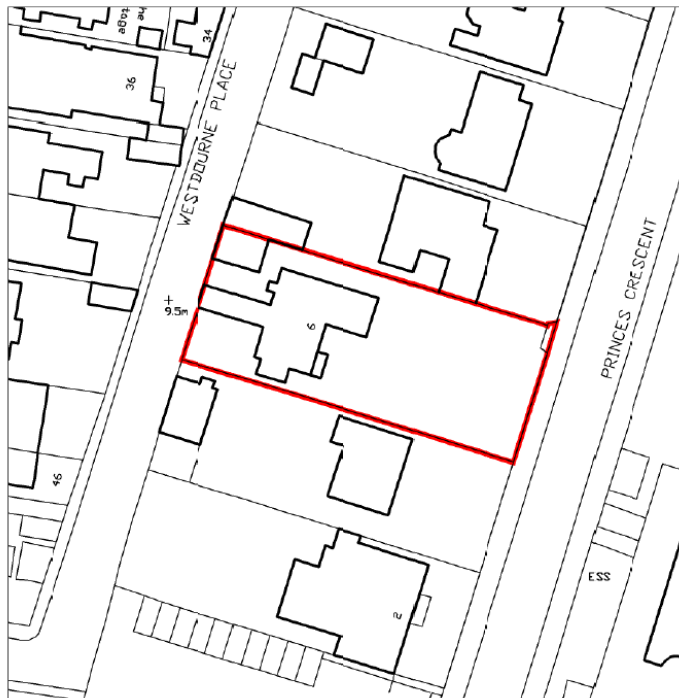
**Brighton & Hove  
City Council**

# Application Description

- Demolition of existing house and erection of new detached two storey dwellinghouse (C3)



# Existing Location Plan



**Site location plan** Scale 1:1250 @A3



Brighton & Hove  
City Council

ID

# Aerial photo(s) of site



Brighton & Hove  
City Council

# 3D Aerial photo of site

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Brighton & Hove  
City Council



# Street photo(s) of site



Front elevation, taken from Princes Crescent



Brighton & Hove  
City Council

# Other photo(s) of site



Front elevation, taken from Princes Crescent



Brighton & Hove  
City Council

# Other photo(s) of site



Front elevation, taken from Princes Crescent



Brighton & Hove  
City Council



# Other photo(s) of site

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Rear elevation, taken from Westbourne Place, facing North



Brighton & Hove  
City Council

# Other photo(s) of site

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Rear elevation, taken from Westbourne Place, facing South



Brighton & Hove  
City Council



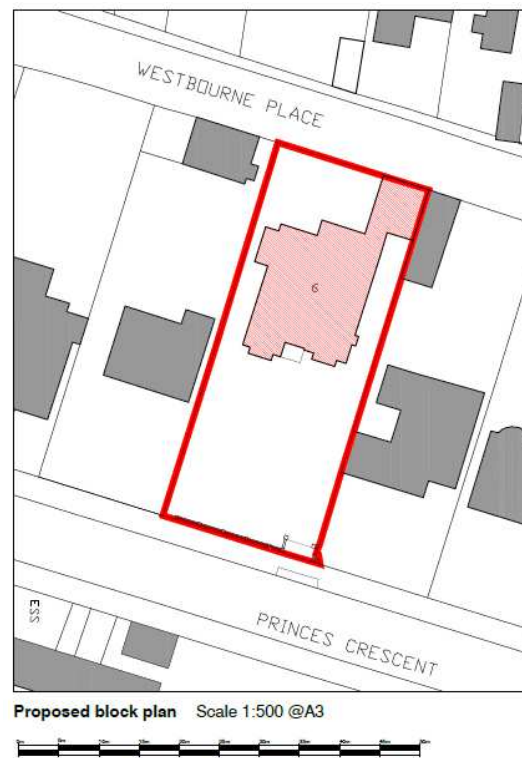
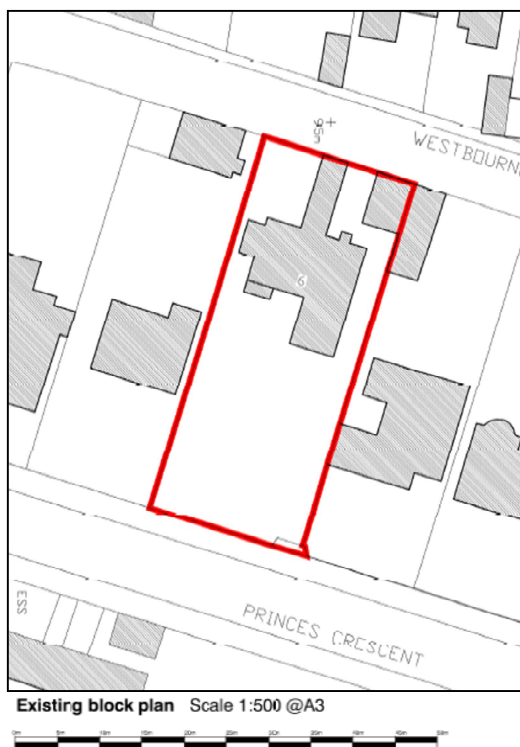
# Map of application site

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# Existing & Proposed Block Plans



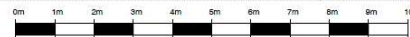
# Proposed Front Elevation



## key

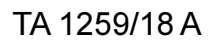
- A conservation type rooflight
- B patent glazing over gallery
- C plain tiled roof
- D facing brickwork to match existing
- E slimline metal windows with bronze colour finish
- F bronze colour metal spandrel panels

Proposed front (east) elevation to Princes Crescent Scale 1:100 @A3



TA 1259/17 A

## 220

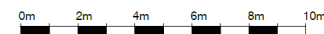




# Existing Contextual Front Elevation



Existing street elevation to Princes Crescent Scale 1:200 @A3

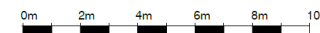


TA 1259/11 A

# Proposed Contextual Front Elevation



Proposed street elevation to Princes Crescent Scale 1:200 @A3



TA 1259/11 A

# Existing Contextual Rear Elevation

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Existing street elevation to Westbourne Place Scale 1:200 @A3

TA 1259/11 A

# Proposed Contextual Rear Elevation

---



TA 1259/11 A

# Comparative Front Elevations

Extant Permission BH2019/03533

Current Proposal BH2020/00776



# Comparative Rear Elevations

Extant Permission BH2019/03533

Current Proposal BH2020/00776





# Comparative North Side Elevations

---

Extant Permission BH2019/03533



Current Proposal BH2020/00776



# Comparative South Side Elevations

Extant Permission BH2019/03533



Current Proposal BH2020/00776



# Key Considerations in the Application

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- The design/appearance of the proposal
- The potential impact on the amenities of local residents
- Impact on nearby heritage assets
- Impact on the adopted highway

# Conclusion and Planning Balance

- The proposal would have a neutral impact on the significance of nearby heritage assets
- The impact(s) on the amenities of local residents has previously been found acceptable
- A building of this scale and general appearance benefits from an extant permission
- The proposal will create significant biodiversity and sustainability improvements

# **Hove Park Nevill Campus, 38 Nevill Road**

**BH2020/00206**



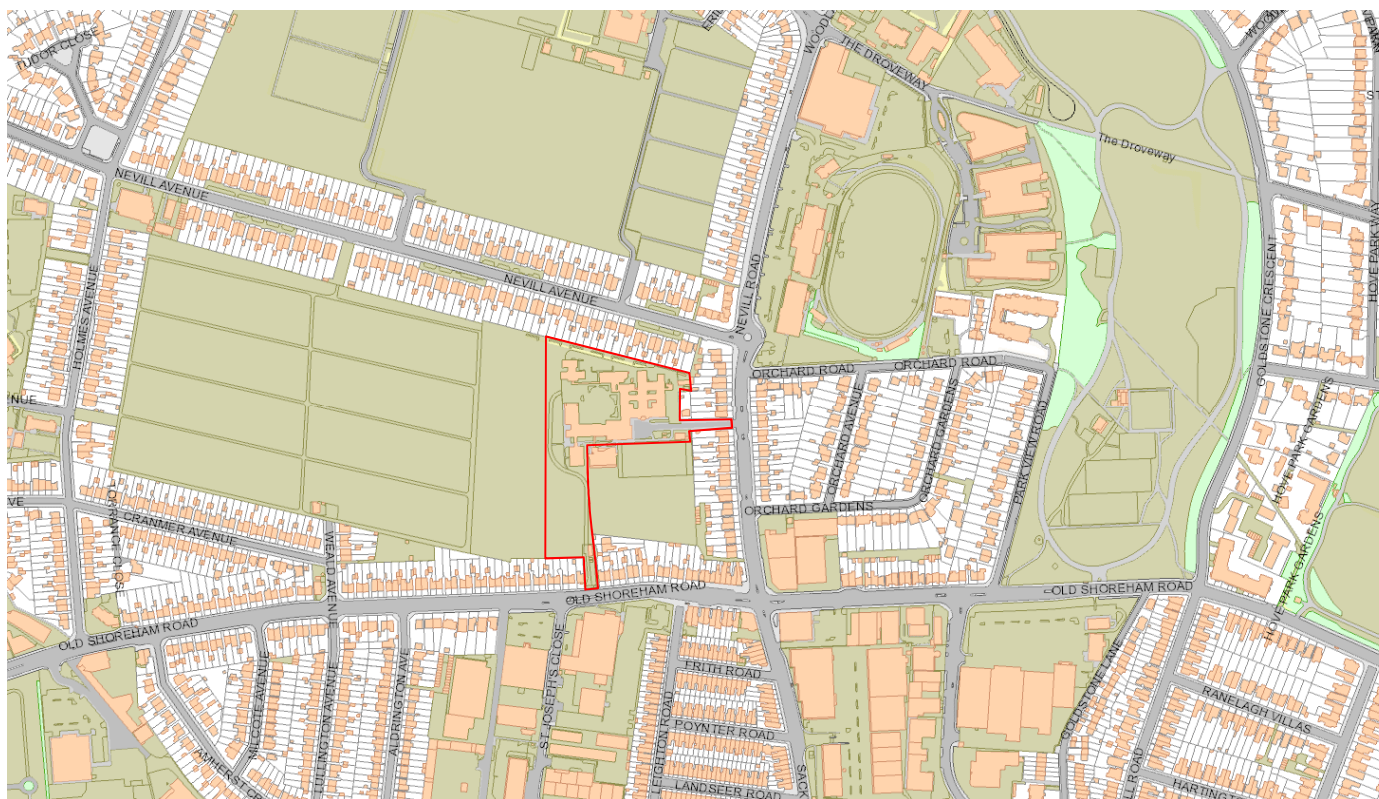
**Brighton & Hove  
City Council**

# **Application Description**

Erection of double height extension to existing gymnasium to create a two court sports hall incorporating part demolition of existing gymnasium & corridor, replacement of existing doorway with window, two new access ramps and refurbishment works.



# Map of application site



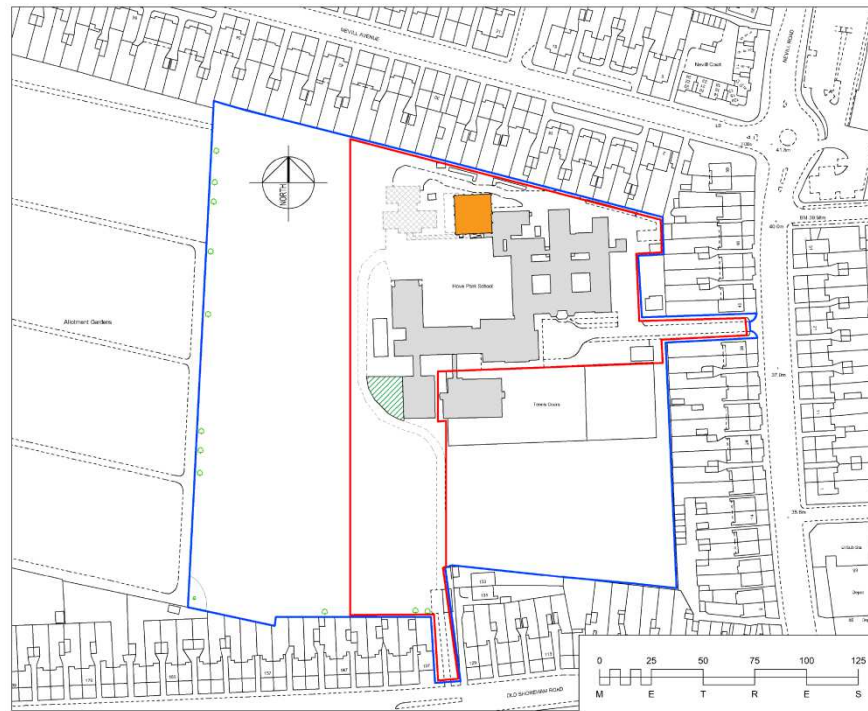
Brighton & Hove  
City Council

# Existing Location Plan



NC-001

# Proposed Location Plan



# Aerial photo(s) of site



Existing gymnasium



# 3D Aerial photo of site from north



Existing gymnasium



Brighton & Hove  
City Council



# 3D Aerial photo of site from south

Existing gymnasium

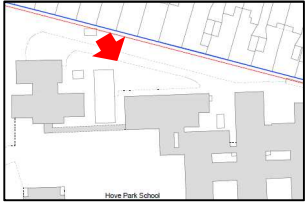


Brighton & Hove  
City Council

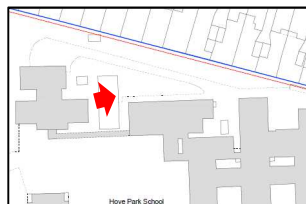


# Photo(s) of site

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# Photo(s) of site



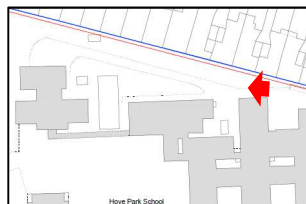
Closest Nevill Avenue  
properties – nos. 16,18,20

Existing gymnasium



Brighton & Hove  
City Council

# Photo(s) of site



Existing dining hall (to be demolished - BH2019/01463)



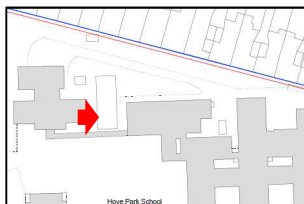
Area to be landscaped to widen road



Brighton & Hove  
City Council



# Photo(s) of site



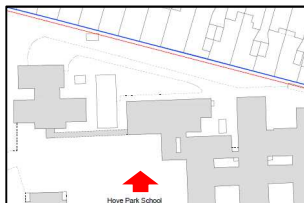
Corridor to be demolished with new access ramp



Brighton & Hove  
City Council

# Photo(s) of site

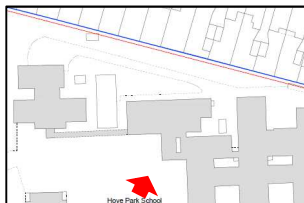
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Brighton & Hove  
City Council

# Photo(s) of site

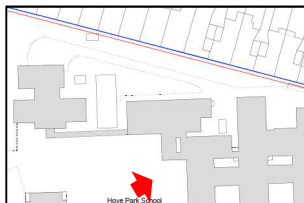
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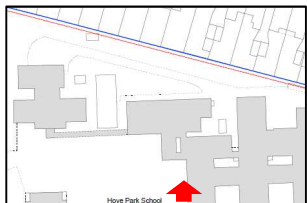
# Photo(s) of site

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Brighton & Hove  
City Council

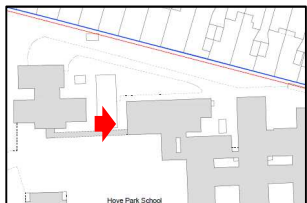
# Photo(s) of site



New access ramp

# Photo(s) of site

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Corridor to be demolished with new access ramp

# Proposed Block Plan

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NC-004

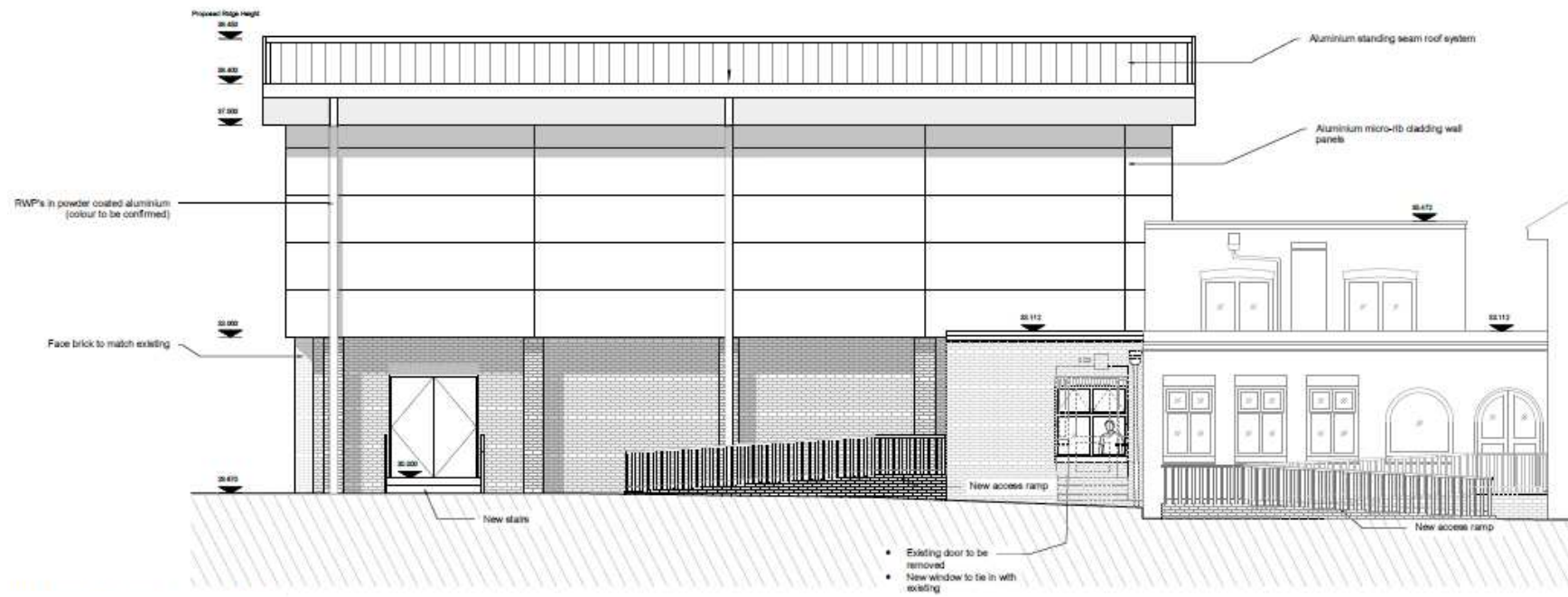
# Proposed North Elevation



NC-012-1



# Proposed South Elevation

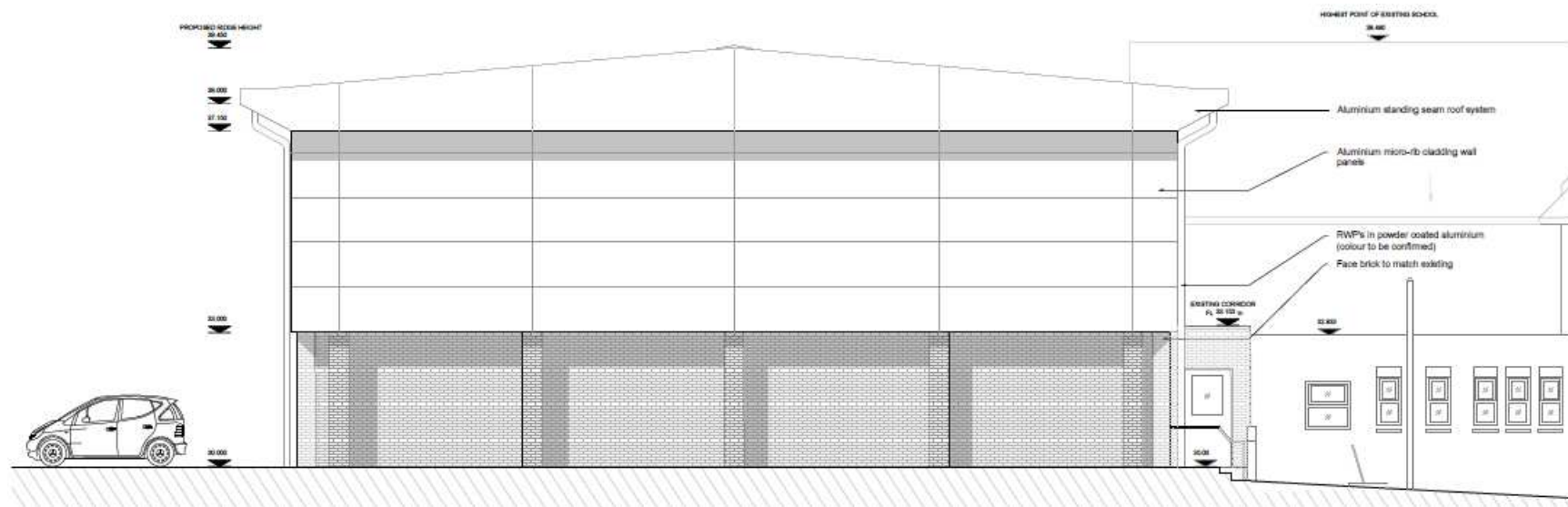


NC-012-2



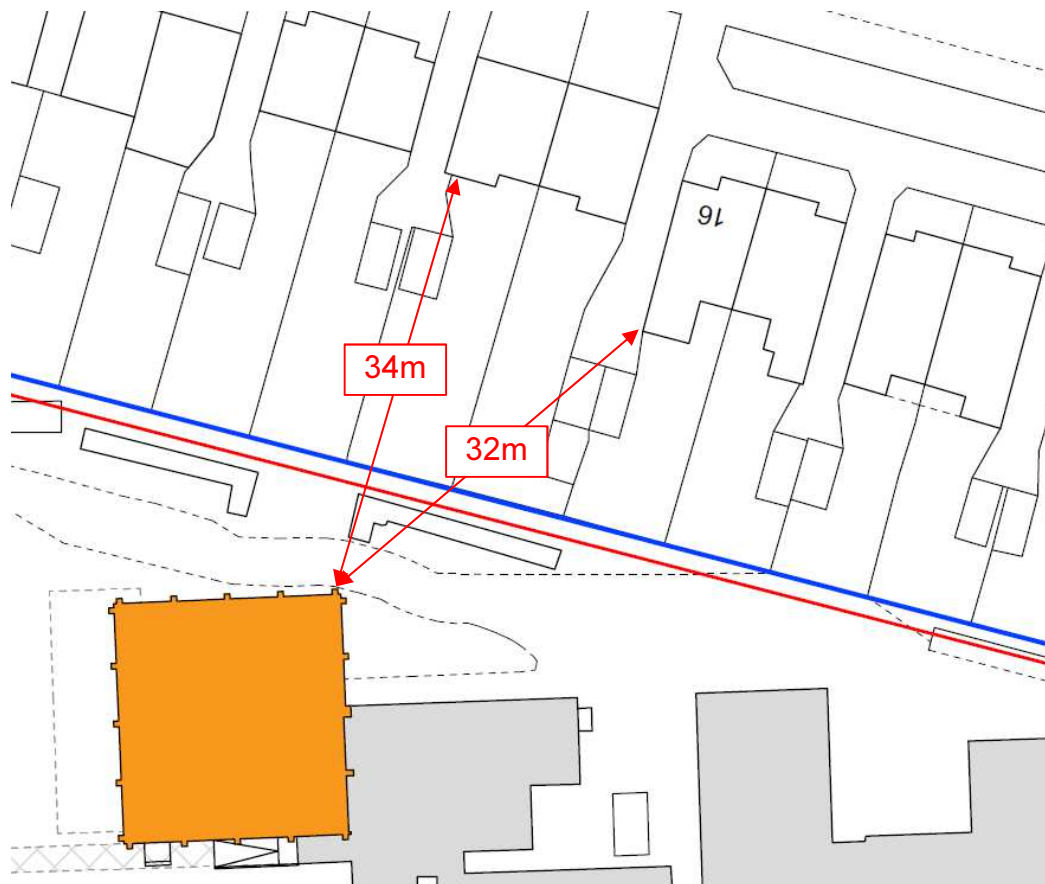


# Proposed West Elevation



Brighton & Hove  
City Council

# Spacing from Nevill Avenue properties



# Key Considerations in the Application

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- Principle of development
- Design & appearance
- Impact on amenity
- Archaeology
- Ecology
- Sustainable transport

# Conclusion and Planning Balance

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- The proposal would enhance the provision of educational facilities on site and would accord with policies CP16, CP18 and CP18 of the Brighton and Hove City Plan Part One;
  - No increase in 'external lets' is proposed with the sports hall being ancillary to the main D1 Education school use;
  - The sports hall would be a large building, however no concerns are held regarding design & appearance;
  - There would likely be some overshadowing of the residential gardens/properties to the north. This would be most noticeable in wintertime at dawn and dusk;
  - Concerns regarding Archaeology, Ecology and waste generated during construction/demolition can be overcome through the recommended pre-commencement conditions;
  - There would be no increase in trip generation as there would be no increase in pupil numbers or 'external lets'.
- 
- On balance, the benefits of the scheme in the provision of enhanced educational facilities are considered to outweigh the potential overshadowing of properties to the north/north-east. Approval is therefore recommended, subject to conditions.





# **Varndean College, Surrenden Road**

**BH2020/00947**



**Brighton & Hove  
City Council**

# **Application Description**

Application for Variation of Condition 2 of approved application BH2018/02404:

Erection of a two storey Science, Technology, Engineering and Mathematics (STEM) centre with associated cycle parking and landscaping alterations.

# Condition 2:

---

## Approved wording:

- The 2 no. temporary classroom structures (nos. 40-72 and 48-50 shown on the site plan 957.18.51revC received on the 28/09/2018) shall be removed from the site within 18 months of the date of this permission or with 3 month of commencement of the use of the STEM building hereby approved, whichever is soonest, and the land returned to its former condition within 2 months of removal.

## Proposed wording:

- The 2no temporary classroom structures (nos. 40-72 and 47 shown on the site plan 957.18.51 Rev.D) shall be removed from the site **within 3 months of the commencement of the use of the STEM** building hereby approved, and the land returned to its former condition within 2 months of removal.



Brighton & Hove  
City Council

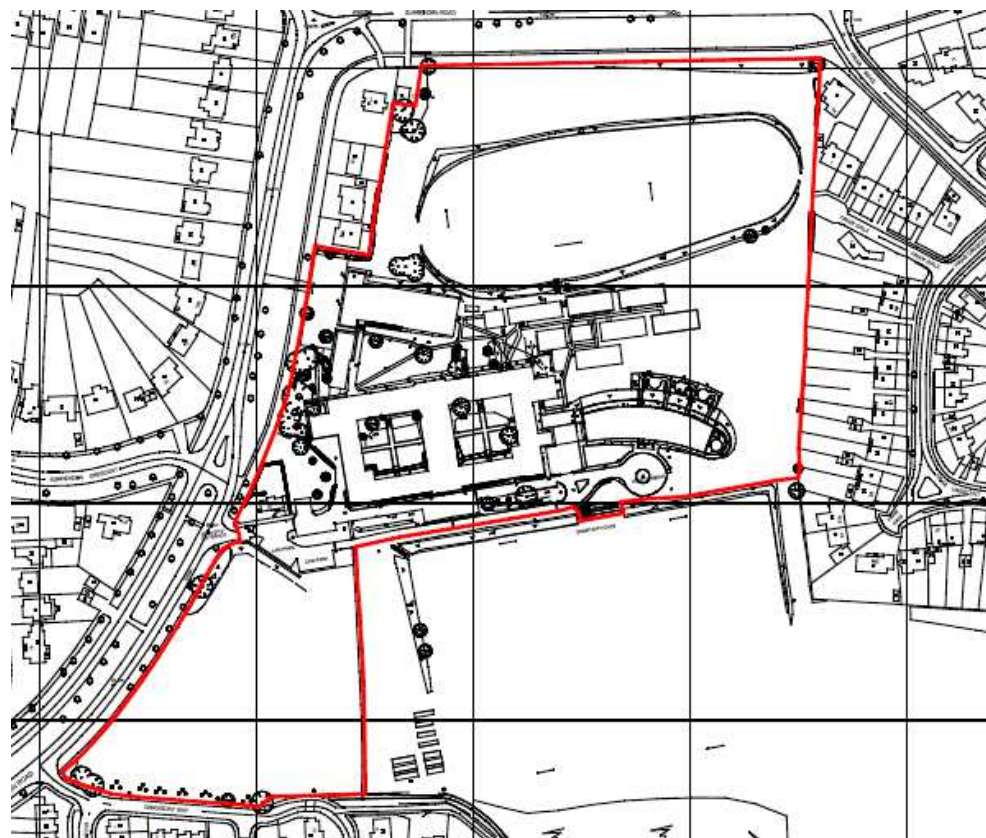
# Proposed Amendments:

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- Removal of temporary classroom 47 instead of temporary classroom 48-50;
- Removal of 2x temporary classrooms within 3 months of the first use of the STEM building - instead of by 17 July 2020 (18 months from date of original permission).

# Site Location Plan

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# Aerial photo(s) of site

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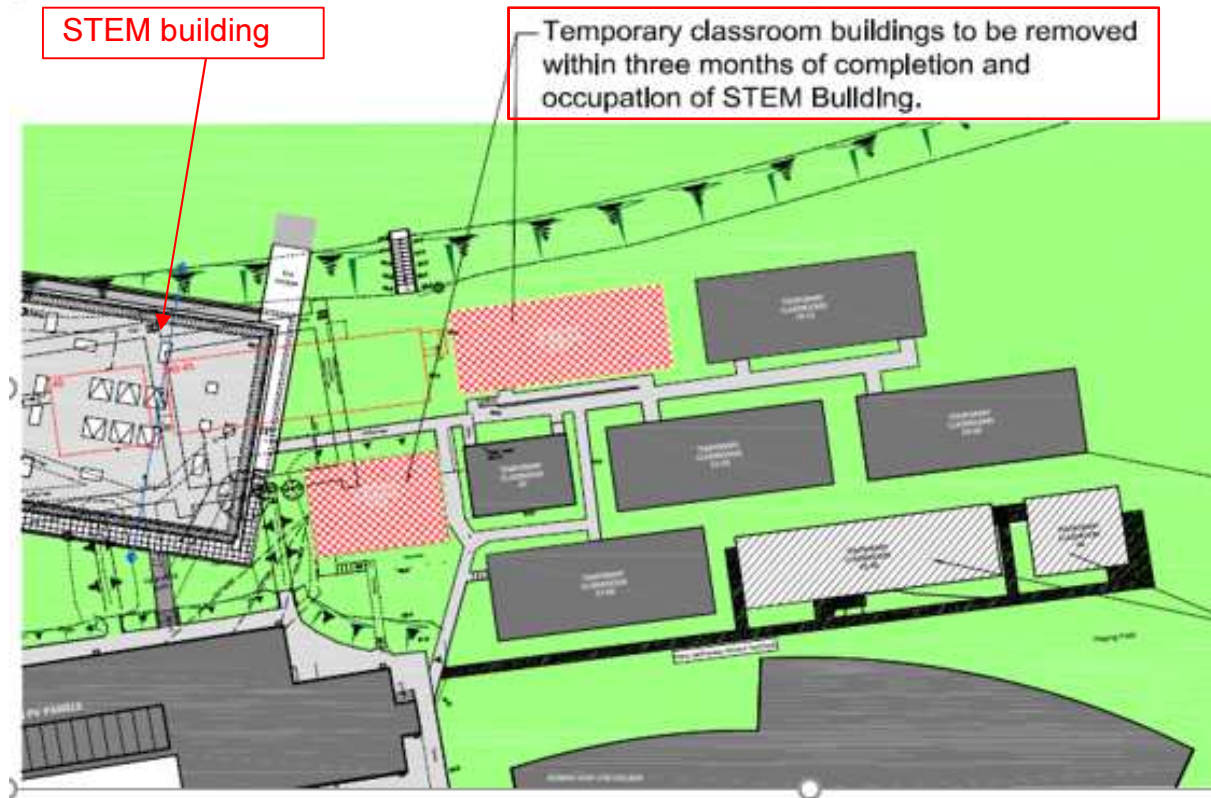


# 3D Aerial photo of site

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# Approved Plan



# Proposed Plan

STEM building

Temporary Classrooms to be removed within 3 months of first use of the STEM building

Remainder of Temporary Classrooms to be removed from site by 19 July 2024



Brighton & Hove  
City Council

# Wider Views of Classrooms



Brighton & Hove  
City Council



# Classrooms to be removed



# Classroom 48-50 to be retained



STEM Building

Classroom 48-50



# Key Considerations

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## Timescales

- The STEM building is now in situ;
- Given the COVID-19 situation, the College is currently closed to most pupils and work on site has ceased;
- The use of the STEM building is therefore unlikely to commence on schedule and removal of the temporary classrooms would be challenging prior to the 18 month deadline (17 July 2020).

# Key Considerations

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## Removal of Temporary Classroom 47 instead of 48-50

- Classroom 47 - Suffering subsidence;
- Classroom 48-50 - Superior condition to other temporary classrooms on site; more efficient in terms of flexible teaching space, general maintenance and energy efficiency;
- Main power supply for entire cluster of temporary classrooms runs through classroom 48-50;
- Therefore removal of classroom 48-50 is logistically challenging.

# **Conclusion and Planning Balance**

## **Timescales**

- Proposal to allow the College more time to remove the temporary classroom buildings is reasonable and justified in the current circumstances.

## **Removal of Temporary Classrooms**

- Given the design, scale and massing of the approved scheme as a whole and taking account of the challenging logistics and the superior condition of classroom 48-50, the amendment to remove classroom 47 instead is relatively minor in nature;
- The development as a whole would remain appropriate to the character of the area, with no impact on neighbour amenity, transport or ecology.

